



July 13, 2022

To: Supervisor Susan Gorin

*Via email:*

Re: Springs Municipal Advisory Council comments on the Springs Specific Plan EIR

Dear Supervisor Gorin,

#### **Our Concerns**

**Once** the Draft Environmental Impact Report (DEIR) is approved, the next step is approval by the Board of Supervisors for one of the proposed Alternative Plans. Opportunities for changes of the Springs Specific Plan (SSP) are essentially over. Given the length of the documents and time taken to produce the SSP and DEIR the tight comment period is inadequate. Our primary concern is the change in economic reality in the Springs since 2015. Intervening structural and economic changes in Sonoma County and the country have caused the economic reality to worsen for our residents.

- **Time** between inception of planning (2015) for the SSP and the final product in 2022. Significant and unexpected changes have taken place in the Springs Community. Wildfires beginning in 2017, inception of the COVID pandemic in 2019 and resulting inflation have caused significant changes. Loss of income for many, increases in the costs of housing and the ability of very low to median income individuals and families to afford housing. Problems already rampant in the Springs Community were intensified.
- **The** SSP addresses the physical aspects of the Springs but not the human composition of the Springs. Particularly climate change, wildfires, and the impact on our population.

**Economic** concerns are primary objections to the plans as they stand. The DEIR and SSP downplay the economic reality of the Springs area. The Planners have classified the effect of the resulting actions caused by plans as 'Less than Significant'. We assure that this is not true for the people of the Springs who will live the reality of which ever plan is chosen.

- Displacement of Springs community members by gentrification is inherent in the plans. As well, displacement will occur as the plans are implemented. Public meetings to provide input for the SSP emphasized the desire and need of the community to avoid displacement of vulnerable



residents and avoid gentrification. Intervening time has worsened economic conditions for vulnerable persons. Ignoring this reality is unacceptable. Actions must be included in the DEIR and SSP that provide robust mitigation measures.

- **Maintaining** a diverse and vibrant Springs population. Our population is ethnically, racially, economically and age mixed. As a community we continue to learn to live with each other and grow from our mutual experiences. We realize that at times our aspirations exceed our actions.

#### **Consider** life in Sonoma Valley without the Springs

- **Economic** health of the entire Valley depends on workers who make their homes here. The jobs of many wage workers do not pay enough for the necessities. It is well documented that most public service workers as well as lower wage earners cannot afford to buy a home.
- **Realistically** residents draining out of the Springs create problems for local businesses in lost revenues due to staffing shortages including business closures due to lack of staffing. This is a contributing factor to declining public school enrollment.
- **We** cannot forget other residents. The non-wage earners—children, stay at home parents, retired people, permanently disabled, extremely low income and homeless persons. We are a whole community. We must consider everyone living in the Springs, no matter their economic reality.

**For** these reasons we challenge Permit Sonoma staff to make provisions to fit the reality of the Springs and our sincere desire for an inclusionary community.

- Suggestions include provisions to maximize participation by experienced low-cost housing developers, emphasize affordability at all stages of planning and construction, encourage modest ADUs, and provide intermediate housing for people displaced from their homes by new construction.

**Last**, but not least. We urge robust attention be turned to open space for community recreation and gathering. In addition, we strongly recommend that Larson Park receive the needed improvements to make it a safe, usable, and enjoyable community space. The Plaza location is indeed historic, although smaller than the original space. What is designated is little more than a parking lot in a commercial area. It is not too much to ask for more space to be found and provision for starting to move forward on a Springs Community Plaza. The residents of the Springs provide economic advantage to the Sonoma Valley and by extension, Sonoma County.

Please see included letter to Caltrans regarding our recommendations for traffic, road, and pedestrian safety.

*The Springs Municipal Advisory Council represents the people of the Springs in Sonoma Valley as the voice of the community to elected representatives. SMAC is committed to engage with all community members in meaningful and inclusive ways to promote the health and wellbeing of the Springs.*

Springs MAC: SSP Comment Letter  
July 13, 2022



Sincerely,



Maite Iturri  
Chairperson  
Springs MAC

CC: Sonoma County Board of Supervisors, Permit Sonoma, Sonoma City Council, North Sonoma Valley  
M.A.C., SVCAC

enclosed: Caltrans Letter

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March 2, 2022

Caltrans  
Sean Nozzari  
Deputy Director of Traffic Operations  
Caltrans Bay Area (District 4)

*Via email:*

**RE: Urgent Request for Improvements to Traffic Safety in the Springs**

Dear Caltrans,

The Springs Municipal Advisory Council (Springs MAC) is aware of ongoing meetings between Supervisor Susan Gorin, CalTrans and Sonoma County Transportation and Public Works (TPW) regarding improvements to traffic safety in The Springs along the Highway 12 (HWY 12) corridor. The CalTrans Complete Streets program requires that the Safety and Accessibility of walking, biking, and taking public transit be prioritized on their roads whenever a project is undertaken. Due to recent improvements on Hwy 12 in the City of Sonoma, we have become aware of the possibilities in additional safety improvements that could be made for pedestrians and bicyclists in our community. We (The Springs MAC members) make an urgent request that CalTrans initiates action at the earliest possible time, on the following requests for pedestrian, bicycle and vehicular safety in the Springs.

**Highway 12/Verano Avenue Intersection/Verano Avenue & Main Street Crosswalk**

We grant that the Hwy 12/Verano Ave intersection is the only portion of this request under Springs (County) and CalTrans joint jurisdiction. CalTrans personnel are aware of the death of two people and two girls in their early teens injured at that intersection. All of these occurrences were within less than a quarter mile of the locations mentioned (see below chart for references). The deaths of two people, one in the City of Sonoma and the other in Sonoma County (the Springs), highlight our awareness that conditions at the Hwy 12/Verano Ave intersection directly impact conditions at these two sites. It is our contention that improvements to the Hwy 12/Verano Ave intersections are highly indicated.

Our concerns are the inequity of designated turn signals and lanes at the Hwy 12/Verano Ave intersection that create confusion and increased danger. The critical point seems to be for vehicles turning left from the west on Verano to proceed north on Hwy 12.

**Recent Injuries and Deaths at Verano Ave and Highway 12 intersection**

Date	Location	Details	Article link





<b>3/22/21</b>	Bike Path— HWY 12 Maxwell Park	Fatality Pedestrian James Pendergast hit by work van	<a href="https://www.pressdemocrat.com/article/news/highway-12-north-of-sonoma-closed-after-fatal-collision/?artslide=0&amp;sba=AAS">https://www.pressdemocrat.com/article/news/highway-12-north-of-sonoma-closed-after-fatal-collision/?artslide=0&amp;sba=AAS</a>
<b>2/14/21</b>	Crosswalk HWY 12 & Verano Avenue between Palms Cafe and McDonald's	Accident Two girls injured by Car	<a href="https://www.sonomanews.com/article/news/2-girls-struck-at-verano-and-sonoma-highway/">https://www.sonomanews.com/article/news/2-girls-struck-at-verano-and-sonoma-highway/</a>
<b>11/24/20</b>	Crosswalk Verano Avenue & Main St	Fatality Pedestrian Ray Carrillo hit by Car	<a href="https://www.sonomanews.com/article/news/pedestrian-dies-after-being-hit-by-car-in-sonoma/">https://www.sonomanews.com/article/news/pedestrian-dies-after-being-hit-by-car-in-sonoma/</a>

### **Springs/HWY 12 Crosswalks**

There is need for additional marked crosswalks throughout the Hwy 12 corridor from El Verano to Agua Caliente. The Springs Community is dense with businesses and housing along that stretch of highway. Many people walk to shopping and other businesses throughout the Springs. Vehicles frequently take advantage of gaps in traffic to speed from stop signal to stop signal making unsafe conditions for pedestrians and bicyclists. There are few actual cross streets (only "T" intersections) in the area, increasing the need for more marked crosswalks. We request implementation of four pedestrian beacons. Three beacons at already marked crosswalks on Highway 12 located at Boyes Foods Center (16205 Sonoma Hwy), El Molino (11 Central Avenue)/Andy's Liquor (18002 Sonoma Hwy), and Parsons Hardware x Waterman (17800 Sonoma Hwy). The additional pedestrian beacon should be placed at the Verano Ave/Main Street crosswalk (TPW area of responsibility).

### **Donald Street Gap, Bike Lanes**

The Donald Street Gap along both sides of the highway, in the area between Happy Dog (18962 Sonoma Hwy) and Baker and Cook (18812 Sonoma Hwy), has no sidewalk. Pedestrians must walk alongside traffic in the bike lane/shoulder. The bike lanes are more like bike gutters, and for pedestrians and biking to be safe, more space and protection than a line of paint needs to be given to both groups. These goals are in line with the Complete Streets Program, and need to be undertaken sooner than later.

The Springs area of Unincorporated Sonoma County is recognized as a disadvantaged community. We are not wealthy. That does not mean we are not worthy. We ask that CalTrans work with TPW and Supervisor Gorin (and the Board of Supervisors, as needed) as she advocates for us, to address the above safety needs along Highway 12. We want to minimize accidents and prevent further pedestrian fatalities.

Sincerely,

[Redacted Signature]

Chair, Springs Municipal Advisory Council

Springs MAC letter

March 2, 2022



CC: Sonoma County Board of Supervisors, Sonoma County Transportation and Public Works, City of Sonoma City Council, Sonoma County Transportation Authority, Permit Sonoma, Governor Gavin Newsom, Congressman Mike Thompson, Congressman Jared Huffman, Senator Bill Dodd, Senator Mike Maguire, Representative Cecilia Aguiar-Curry, Senator Diane Feinstein, Senator Alex Padilla

Please see attached community petitions along with reports:

- springs\_hwy\_12\_ladg.pdf
- CBTP The Springs (Central Sonoma Valley)2010.pdf





# Tell CalTrans -- Verano x Hwy 12 Needs Safety Improvements / 12 x Verano es Peligrosa

To: CalTrans District 4, City of Sonoma, Sonoma County Board of Supervisors, Vision Zero Sonoma County

Two men have died and two girls have been injured at the intersection of Highway 12 and Verano and surrounding areas since November of 2020. Anyone who drives, walks, or bikes through this area knows it is dangerous. This intersection is under the jurisdiction of various agencies, which seems to complicate the matter. We NEED these agencies to work together to prevent and minimize future injuries and deaths on our streets.

Spanish translation / traduccion en español:

Dos hombres murieron y dos jovencitas resultaron heridas en la intersección de la autopista 12 y Verano Avenida y las áreas circundantes desde noviembre de 2020. Cualquiera que maneje, camine o ande en bicicleta por esta área sabe que es peligrosa. Esta intersección está bajo la jurisdicción de varias agencias lo que parece complicar el asunto. NECESITAMOS que estas agencias trabajen juntas para prevenir y minimizar futuras lesiones y muertes en nuestras calles.

## Why is this important?

People are dying and getting hurt while just trying to walk.

Spanish translation / traduccion en español:

Las personas mueren y se lastiman mientras intentan caminar.

This petition will be delivered attached to a letter outlining the issues with this intersection.

Spanish translation / traduccion en español:

Esta petición se entregará adjunta a una carta que describe los problemas con esta intersección.

Signed by 208 people:

Name	Zip code
Hannah Perot	95476
Gabriel Bassett	95476
ChristineChristine Talbot	95476
Lisa Lee	95476
Patrick Hanlon	95476
Gabrielle Guinea	95476
Toni Shweiky	95476
Leksi H	95476
paul maysonave	95476

<b>Name</b>	<b>Zip code</b>
Ana Paez	95476
Daniel Levitis	95476
Dana Bravo	95476
Peggy Allan	95476
Annette Winston	95476
Bruce Winston	95476
Jane Schwarz	95476
Ann Marie Paparelli	95476
Tiffany Bell	95476
Ana Ortiz	95476
Nomi Yah	95476
Melissa Townsend	95476
Loraine Davis	95476
Alba Terri	95476
Mara Kahn	95476
Bret McIntyre	95476
Johanna Winship	95476
Alyssa Dieterich	95476
Robin Jones	95476
Melinda Moore	95476
Catherine Grande	95442
Cheryl Shimek	95476
Valerie Helton	95476
Linda Bryan	95433
Shelley Carlisle	95476
Anastasia Encarnacion	95476
Sara Nicholls	96476
Gaye Freeman	95476
Shirryl Bayless	95476
Sara Parker	95476
Georgeann Quinn	95476

<b>Name</b>	<b>Zip code</b>
Emmy Kaplan	95416
Ken Rogers	95476
Judy Hall	95476
Robin Hardisty	95476
Jochen Hammerschmidt	95476
Michelle Quiñonez	95476
Mark Janofsky	95476
Rhonda Bonecutter	95476
Janet McLaughlin- Davis	95476
Barbara Merle	95476
Sonia Ponce	95476
Kristen Garneau	95476
John Hayes	07546
Vicki DeSmet	95476
Katie Vormelker	95476
Susan Chauncy	95409
Juana Hernandez	95476
Kara Reyes	95476
Marisela Martinez	95476-3626
Kari Bounds	95476
Adrienne Garrison	95476
Araceli De Jesus	95476
Amy Cox	95476
Gardner Smith	95476
Michael Acker	95416
Laurie Oleary	95476
julie calhoun	95476
Carol Eldridge	95476
Dianne Partington	95476
Donna Baker	95442



<b>Name</b>	<b>Zip code</b>
Abbey Payne	95476
Julia Mason	95476
Katie Roberson	95476
Garry Baker	95476
Amber Smith	95476
Jack Allan	95476
Sharon Langord	95476
Christina Sanders	95476
Erica Brumley	95476
Crystal Zelaznowski	95476
Jill Gade	95476
Marnie Sauls	95476
jim gade	95476
Janice Folzman	95476
Mary Jue	95476
Renee Willis	95476
Nancy Evers Kirwan	95476
christy guidry	95401
Erin Tracy	95476
Susan Tucker Innamorati	95476
Cate mcdonough	95476
Jeannette Scharich	95476
Jaden Justice	95476
Jill Pressley	95476
Willow Taraja	95476
Renea Magnani	95476
Claudia Trujillo	95442
Cynthia Lund	95476
Cynthia Mednick	95476
Sandra Zimmermann	95476

<b>Name</b>	<b>Zip code</b>
Elizabeth Powers	95476
Steve Caniglia	95476
Tony Ginesi	95476
Erica Tuohy	95476
Nicole Holley	95476
Laura Brintnall	95476
Margaret Lourenco	95476
Gail Macaluso	95476
Craig Duncan	95476
Michele Wagner	95433
Michael Oleary	95476
Jessica Rose	95476
Jacob Mikow	95476
Rhodd Crystal	95476
Frances Mccormack	95476
Sharon Garrow	95476
susan lucas- conwell	95476
Stephen Smart	95476
Tim Deschenes	95476
Rich Lee	95476
Carolina Hernandez	95476
Jim Vides	95476
Elizabeth Willett	95476
Elizabeth Skrondal	95476
Alyssa Conder	95476
Barbara Jacobsen	95476-6840
Anoushka Donnelley	95476
Mary Kay SEVIER	95476
Pam Bernard	95476

<b>Name</b>	<b>Zip code</b>
Susan Smart	95476
Aida Ramírez	95476
Cynthia Carr	95476
phil budge	95476
John MAZZA	95476
Jessica Montgomery	95476-7376
Shannon Rossiter	95476
Allison Stieper	95476
David Prete	95476
Joan Bourg	95476
Lorelei Farmer	95476
Kraig Meyer	95476
Michael Schilling	96576
Maureen Feldman	95409
Dan Grgich	95476
Jon L Foreman	95476
Denise Halbe	95476
Lance Robbins	95476
Sheila Zak	95433
Carol Foreman	95476
Robert Steingraber	95117
Magdalena Ortega	95476
Ciji Johnson	95476
Jenny Perez	95476
DiAnne Durossette	95476
Melissa Keylon	95476
Cynthia Hamilton	95476
Roger Bassett	95476
Carrie Taylor	95403
Joanne Fusco	95476



<b>Name</b>	<b>Zip code</b>
Denise Salvo	95476
Carol Cenci	95476
Joan Fabian	95476
Sarah Parker	95476
Melanie Luque	95476
Joyce Winters	95304
Chenin Williams	95404
Paige Brooks	95476
Kathleen Geissinger	95476
Amy Smith	95476
Kelli Jackson	94931
Anna Mafort Lacy	93446
Sally Houghton	95476
Randall Bounds	95476
Michele Duckett	94513
Mara Roche	95476
Blake Petersen	95476
Sandra Otter	95476
Christopher Rateaver	95433
Nick Roualdes	95476
Stacy Simonson	95476
Betty Gilliam	95476
Walker Candace	95476
Brandy Melendy	95442
Mary Claudias	21228
Kara Villanelli	19808
Monica Badgley	95476
Jennifer Cassino	95476
Michele Repetto	95476
Deanna Johnson	95476
Matthew Ogburn	21403

<b>Name</b>	<b>Zip code</b>
Shannon McCambridge	95476
Chandra Seelye	93940
Amber Dawson	95451
Julia Allen	94559
Kristine Benson	95476
Paul Benson	95476
Elena Whiteley	95476
Kristina Sharp	95476
M Smith	95476
Linnea Laufer	95476
Sarah Sasse	95476
Carly Fedor	95442
Dave Leyva	95476
Keith Bongiovanni	95476
Eva Valfer	95486
Frederick Allebach	95487
Pamela Burns- Clair	95476
Kimberly Cameron	95476



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# THE SPRINGS

## COMMUNITY BASED TRANSPORTATION PLAN

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JUNE 14, 2010

**SCTA**  
sonoma county transportation authority



## ACKNOWLEDGEMENTS

### **Stakeholders Committee**

Mike Acker	Zuli Baron	Mario Castillo
Ellen Conlan	Stephen Dale	Father Donahue
Edwin Ferran	Kevin Howze	Rich Lee
Al Lerma	Kara Reyes	Chip Roberson
Cynthia Scarborough	Steven Schmitz	Jerome Smith
Kenneth Tam	Jennifer Yeaman	

### **The Public and Interviewees**

Sincere thanks are given to all who completed surveys, gave their opinions, and participated in interviews. The public input was crucial to the integrity of this plan.

### **Sonoma County Transportation Authority (SCTA)**

The plan was prepared by SCTA, whose Directors are:

Valerie Brown	Laurie Gallian	Pat Gilardi
David Glass	Sarah Gurney	Paul Kelley
Jake Mackenzie	Mike McGuire	Carol Russell
Sam Salmon	Gary Wysocky	Shirlee Zane

### **SCTA Staff**

Executive Director, Suzanne Smith

Deputy of Planning & Public Outreach, Janet Spilman

Transportation Planner, Project Lead, Lynne March,

Transportation Planner, GIS Mapping, Chris Barney

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# EXECUTIVE SUMMARY

*The Springs Community Based Transportation Plan* focused on outreach to residents of the study area to involve them in the identification of transportation problems and potential solutions. This plan was funded by the Metropolitan Transportation Commission (MTC) and conducted by the Sonoma County Transportation Authority (SCTA). Part of central Sonoma Valley was identified as a “Community of Concern” by MTC, based on the percentage of low-income residents living there. The study area was further defined to include a population of approximately 12,000. The purpose of the plan is to identify options for improving transportation for the area’s low-income population.

To provide context, the plan includes a brief area history; demographic information; description of existing conditions and services; highlights of future plans; description and analysis of transit services; and detailing of the utilized outreach strategy. The key components of the plan, however, are the public outreach findings and actionable solutions derived from them.

The most common concerns of the area’s low-income residents are pedestrian safety and being able to reach destinations without driving a vehicle.

Availability of transit services and access to safe bicycling and walking facilities are, therefore, key.

Twenty-three “solutions” have been proposed to improve transportation safety, mobility and access for residents of The Springs. These projects and strategies correspond to community-identified transportation needs. The plan also provides information about the problems and associated barriers to implementation.

There is value in having long-range plans in place to provide guidance regarding public priorities, and to offer ideas to the public and private sectors about projects and strategies that could be implemented over time to improve transportation for the target population.

SOLUTIONS SHOWING RANKING		
Highest Priority		
A	Increase frequency of route 32 buses to/from The Springs and Sonoma	1
B	Safe Routes to Schools program	1
C	Complete the Central Sonoma Valley Bikeway (Class I, multi-use path)	2
D	Bicycle Education Campaign & Street Skills classes	2
E	Maintain existing levels of transit service	3
F	Enhance pedestrian crossings on Highway 12 at various locations	3

SOLUTIONS SHOWING RANKING		
G	Install more shelters, benches & bike racks at bus stops	3
H	Expand outreach & customer service efforts to potential & existing Latino bus patrons	3
<b>Medium Priority</b>		
I	Increase frequency of route 40 buses to/from The Springs & Petaluma, including Saturday service	4
J	Increase frequency of route 30 buses to/from The Springs & Santa Rosa & Sonoma	4
K	Complete Verano Avenue sidewalks from Highway 12 to Sonoma Creek	4
L	Arnold Drive bicycle lanes from Agua Caliente Road to Country Club Drive	4
M	Implement a new weekday bus route between the cities of Sonoma & Napa	5
N	Agua Caliente Road bicycle lanes from Highway 12 to Arnold Drive	5
O	Boyes Boulevard sidewalks from Highway 12 to Arnold Drive	5
P	Boyes Boulevard bicycle lanes from Highway 12 to Arnold Drive	5
<b>Lower Priority</b>		
Q	Later afternoon and/or evening bus service & expanded ADA paratransit service	6
R	Pedestrian lighting on Highway 12 from Donald Street to Verano Avenue	6
S	Agua Caliente Road sidewalks from Highway 12 to Vailetti Drive	6
T	Provide incentives for businesses to provide safe & convenient bicycle parking	6
U	Reinstitute Golden Gate Transit route 90 bus service from Sonoma Valley to San Rafael & San Francisco	7
V	Add pedestrian crossings on Verano Avenue at Riverside Drive	7
W	Pedestrian lighting Agua Caliente Road & Boyes Boulevard	7

## CHAPTER 1

# OVERVIEW

## PLAN INTRODUCTION & PURPOSE

*The Springs Community Based Transportation Plan* is a transportation plan based on community input. Funded by the Metropolitan Transportation Commission (MTC) and conducted by the Sonoma County Transportation Authority (SCTA), the plan emphasized community outreach to ensure a collaborative process inclusive of residents; employers; community-based and faith-based organizations; transportation and service providers; governmental agencies; and the business community. The planning process focused on outreach to low-income residents of the study area, and involved them in the identification of both transportation problems and potential solutions. Adequate options for mobility are important for ensuring access to employment, health care, education, recreation and community destinations. This plan provides guidance to decision makers in both the public and private sectors as to how the target population's means of transportation could be improved.

## REGIONAL PLANNING

MTC is the San Francisco Bay Area's Regional Transportation Planning Agency (RTPA), as well as Metropolitan

Planning Organization (MPO), and as such has region-wide responsibilities to plan, finance and coordinate transportation. MTC's Community Based Transportation Planning Program was established in 2002 to advance the findings of two reports completed as part of the *2001 Regional Transportation Plan*.

- *The Lifeline Transportation Network Report* identified transit needs in economically disadvantaged communities throughout the nine-county San Francisco Bay Area and recommended community-based transportation planning as a first step to address those needs. The report identified a Lifeline Transportation Network of transit routes and where gaps existed in that network. The report sought to answer: a) Where low-income communities exist; b) What destinations are crucial for low-income people; c) How well public transportation was meeting those needs; and d) How deficiencies could be addressed. The report also recognized that transit may not be the only answer; rather a multi-modal approach was recommended. Other strategies mentioned in the report included vanpools, guaranteed ride-home programs, auto loan programs, community shuttles, dial-

a-ride systems, expanded use of taxi vouchers, modified use of paratransit, and bicycle and pedestrian facilities, including enhanced access to transit.

- *The Environmental Justice Report*, likewise identified the need for local planning in low-income and minority communities. Transportation was acknowledged to be a critical component of economic well being. The report called for community members and service providers to work cooperatively to determine how services could be improved to meet community needs.

By means of the *Equity Analysis Transportation 2030* report, MTC subsequently defined areas they called “Communities of Concern,” identifying which communities were priorities for such planning. MTC determined where there were concentrations of minority and low income populations. Low income communities were defined as those in which thirty percent or more of the households earn below 200% of the federal poverty level. For purposes of this planning, the Federal Poverty Level was doubled to account for the high cost of living in the San Francisco Bay Area. For example, in 2009 the federal poverty level was an income of \$14,570 for a family of two and \$22,050 for a family of four. At 200%, this would be \$29,140 for a family of two and \$44,100 for a family of four. Income thresholds vary according to how many people are in a household (see table below).

THE 2009 FEDERAL POVERTY GUIDELINES FOR CALIFORNIA		
Persons in Family	Federal Poverty Level	200% of Federal Poverty Level
1	\$10,830	\$21,660
2	\$14,570	\$29,140
3	\$18,310	\$36,620
4	\$22,050	\$44,100
5	\$25,790	\$51,580
6	\$29,530	\$59,060
7	\$33,270	\$66,540
8	\$37,010	\$74,020

*For families of more than eight, add \$7,480 for each additional person*

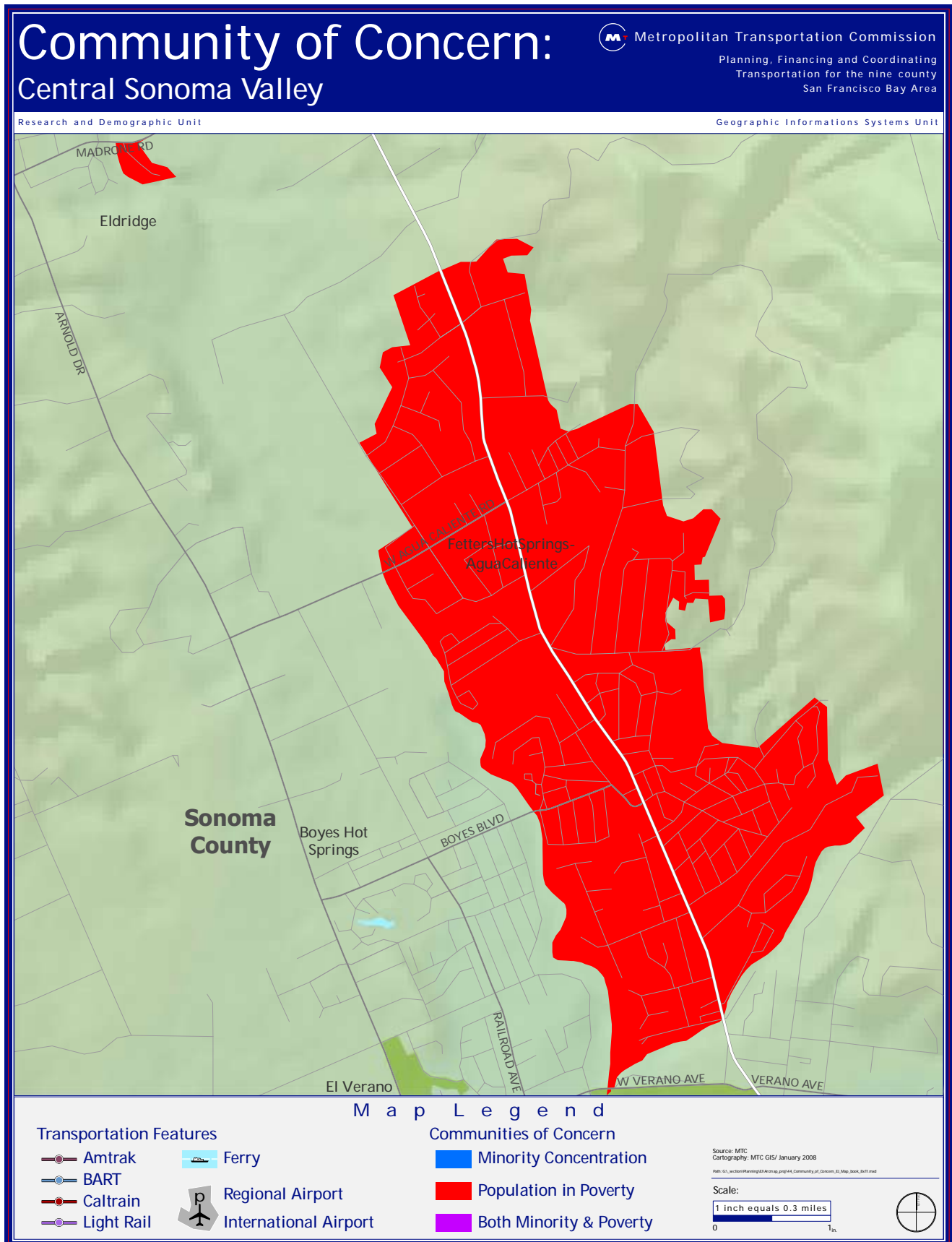
For purposes of the “Communities of Concern,” minority communities were defined as those with seventy percent or more of household occupants being African American; Asian American; Hispanic or Latino; American Indian or Alaskan Native; Native Hawaiian or other Pacific Islander; or Multi-Racial.

Four “Communities of Concern” were identified in Sonoma County based on low-income status (none for minority status based on the 70% criteria). These were named: 1) Central Sonoma Valley, 2) South-Central Santa Rosa, 3) Southwest Healdsburg, and 4) Guerneville/Monte Rio. South-Central Santa Rosa was further identified as the Roseland community. SCTA conducted the MTC-funded Roseland Community Based Transportation Plan (Roseland CBTP), which was adopted by SCTA in June 2007. In 2008, MTC authorized funding to complete eighteen additional CBTPs, including plans for the three remaining “Communities of Concern” in Sonoma County. CBTPs for the Lower Russian River and parts of Healdsburg were adopted in 2009. This plan is the last of the identified four to be prepared for Sonoma County. (Please see Central Sonoma Valley Community of Concern map on page 5.)

## SONOMA COUNTY TRANSPORTATION AUTHORITY

The Sonoma County Transportation Authority (SCTA) is leading CBTP planning efforts in Sonoma County. SCTA acts as the countywide planning and programming agency for transportation—advocating for and securing funding, overseeing projects, and planning for the future. Formed by 1990’s legislation, SCTA is governed by a twelve-member Board of Directors comprised of one elected official from each of the County’s nine city councils and three elected officials from the County’s Board of Supervisors.





SCTA's mission is:

*As a collaborative agency of the cities and County of Sonoma, we work together to maintain and improve our transportation network. We do so by prioritizing, coordinating, and maximizing the funding available to us and providing comprehensive, countywide planning. Our deliberations and decisions recognize the diverse needs within our county and the environmental and economic aspects of transportation planning.*

## CHAPTER SUMMARIES

### Chapter One: Overview

Introduces the plan and its purpose, providing background on origins of the plan and agencies involved in funding and conducting it.

### Chapter Two: Setting & Conditions

Describes existing conditions, including the area's demographics; historical context; existing transit, transportation and other related services; employment, and infrastructure; and provides information about plans and future projections.

### Chapter Three: Outreach Strategy

Documents the outreach process utilized as integral to CBTP planning, including parties involved and strategies used to gain public input.

### Chapter Four: Identification of Problems & Potential Solutions

Details community-identified problems and potential solutions arising from community-based outreach.

### Chapter Five: Action Plan for Implementation

Lays out an action plan based on a prioritization of solutions. Projects and strategies are linked to problems and then described with costs, potential funding sources, agency implementation responsibilities and delineated implementation issues.

## CHAPTER 2

# SETTING & CONDITIONS

## THE SPRINGS STUDY AREA

The Springs area is located in the south-east part of Sonoma County, which is part of the nine county Bay Area region. Named for the area's underground mineral springs, it is located just north of the city of Sonoma and about twenty-eight miles southeast of Santa Rosa, the County's largest city and government center. Sonoma Creek runs north to south through the study area—bisecting it and creating transportation obstacles and opportunities. Sonoma Valley is a premier viticulture and winemaking region and the agricultural lands nearest the study area are mainly vineyards. (Please see base map on page 8 showing the study area location within Sonoma County. The map on page 25 shows the study area parameters).

For purposes of this plan, the study area includes the central area of the communities of Aqua Caliente, Feters Hot Springs, Boyes Hot Springs and El Verano—comprising most, but not all, of the central Sonoma Valley area immediately north of the city of Sonoma. Its parameters were set based on where the majority of the area's low-income individuals and families reside, however, it is significant to note that low-income residents often live in homes mixed within, or not far from, higher-income residences. The study

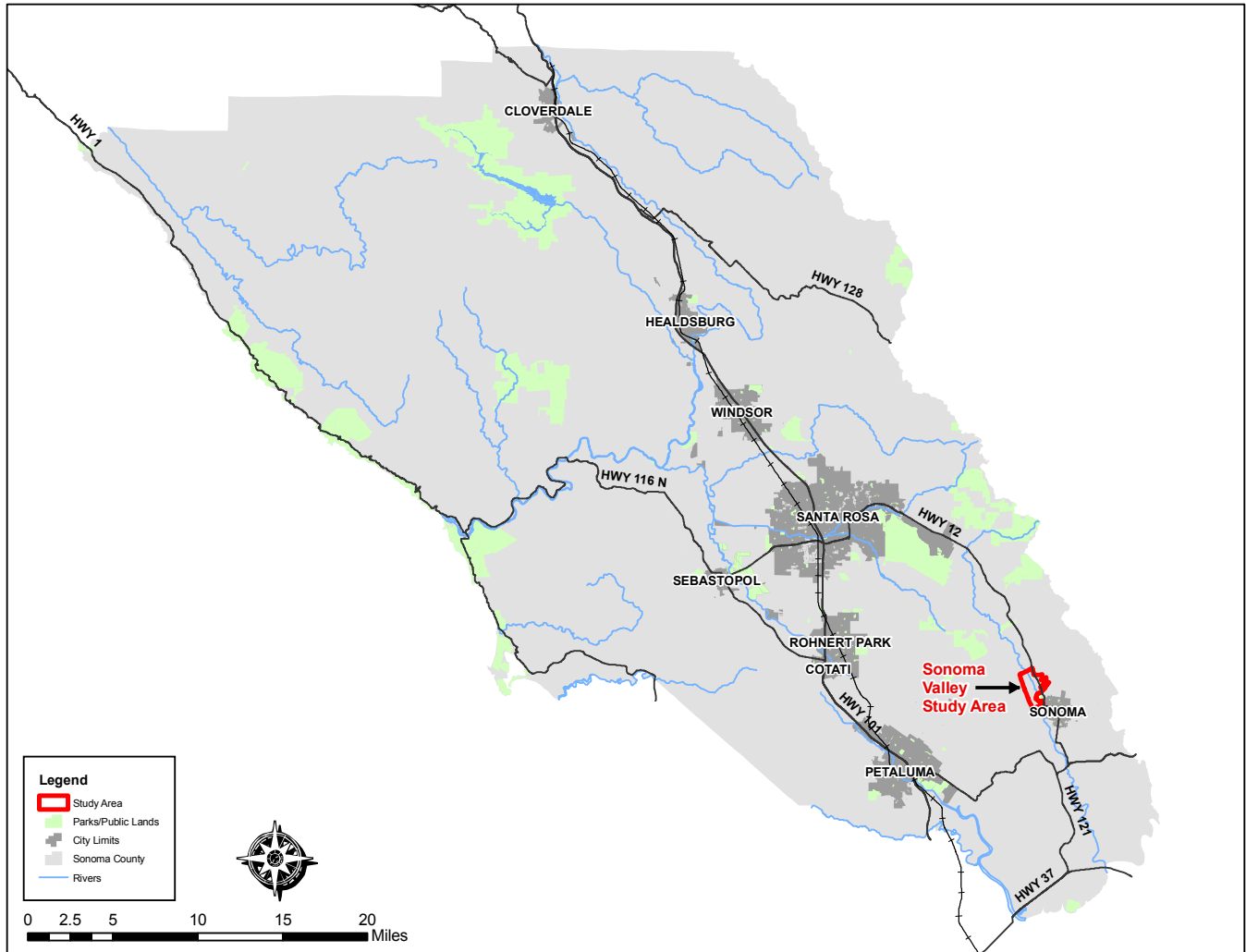
area with a population of 12,265 people is comprised of eight Census Block Groups (CBGs). A particular CBG may include residents of varying incomes. Thirty-three percent of the study area population, which is made up of eight CBGs, live in poverty according to the CBTP definition—with thirty-six percent of that population being Latino. In contrast, the city of Sonoma has a total population of 9,128, with less than seven percent designated as Latino/Hispanic (2000 Census).

## HISTORICAL CONTEXT

### Indigenous Peoples, Mexican Mission and Ranchos

It is unknown exactly when indigenous people first occupied the area, but it is thought that various tribes inhabited the area over a long period. Before the missions came into being there were native villages of thatched huts along Sonoma Creek. Decimating diseases virtually eliminated the native population by the 1880s.

Franciscan Padre Jose Altimira came to Sonoma Valley in 1823. Shortly thereafter Mission San Francisco Solano de Sonoma was established. Lands around the mission were used for growing grain, and tending



livestock—supporting other missions of the region with food. In 1834 an order of secularization was issued by Mexico to divide mission properties among new converts and retain only parish churches. The Mexican rancho era began with subdivision of land into land grants, however grants were mostly given to family and friends, or as rewards for services and favors. Lieutenant Mariano Vallejo was appointed military commandant and director of colonization of the northern frontier; and put in charge of the mission.

The study area was part of two large land grants of General Vallejo. The Petaluma Rancho grant extended twelve miles from Petaluma Creek to Sonoma Creek. The Agua Caliente grant was ten miles long and extended up the valley from

Sonoma Creek on the west to the hills on the east. In the mid 19th century The Springs area was a tourist destination and access to the area was significantly improved with the advent of trains. By 1890, Sonoma Valley Railroad Company had a line with Glen Ellen as the terminus. Verano (later El Verano) was developed by the Verano Land Company as a new town, and promoted by the Santa Rosa and Carquinez Railroad, which built a new depot there. Land was auctioned in 1888, with lots selling for as little as eighty dollars, but development fizzled. By then Sonoma Valley's entire population had reached 3,000.

## 20th and 21st Centuries

At the turn of the century, Northwestern Railway and Southern Pacific Railway promoted tourism to The Springs and the area grew to accommodate visitors. Later automobiles would replace trains as the primary means of travelling to the area.

As with many California areas, the Sonoma Valley population experienced significant growth post World War II. Beginning in the late 1940s and 1950s, and accelerating in the 1960s and 1970s, the entire valley population grew from about 20,000 in 1960 to about 40,000 by 1980. Growth in the percentage of Latino residents has risen steadily, especially since the 1970s. In part because of the greater affordability of an older housing stock and in part due to proximity to vineyard and winery employers, many Latinos have made The Springs their home. The majority of foreign-born Latinos in The Springs area have come from the Mexican states of Jalisco, Michoacan, and Guerrero—and more recently increasingly from the more southerly states of Oaxaca and Chiapas, and Central American countries.

### Historical Context References:

The Sonoma Valley Story, Robert M Lynch,  
The Sonoma Index-Tribune, Inc, 1997

Sonoma Valley Visitors Bureau website

## REDEVELOPMENT AREA DESIGNATION

The Springs Redevelopment Area with a population of about 4,000 people is mostly within the boundaries of this plan's larger study area. The redevelopment area extends from Agua Caliente Road to south of Verano Avenue taking in the area most proximate Highway 12 and including another swath on both sides of Boyes Boulevard two-thirds of the way to Arnold Drive from Highway 12.

The Sonoma County Board of Supervisors approved the Redevelopment Plan for the Sonoma Valley Redevelopment Project in 1984. The area is designated as a redevelopment area under California

Redevelopment Law. The overarching goal of redevelopment is to alleviate the area's physical, social and economic blight. Redevelopment is aimed at improving health, safety, and quality of life in a designated project area. It also focuses on the preservation and expansion of employment and affordable housing opportunities. The redevelopment authority creates a mechanism for reinvesting local property tax revenues into community development projects.

The governing body of the Sonoma County Redevelopment Agency is the Sonoma County Community Development Commission (CDC). Springs redevelopment is advised by the Sonoma Valley Redevelopment Advisory Committee (SVRAC, more commonly just called "the RAC"), which is authorized by the Sonoma County Board of Supervisors to guide redevelopment efforts and charged with ensuring that redevelopment is based on local input. The process involves interaction of the SVRAC with other members of the local community, elected officials and County staff.

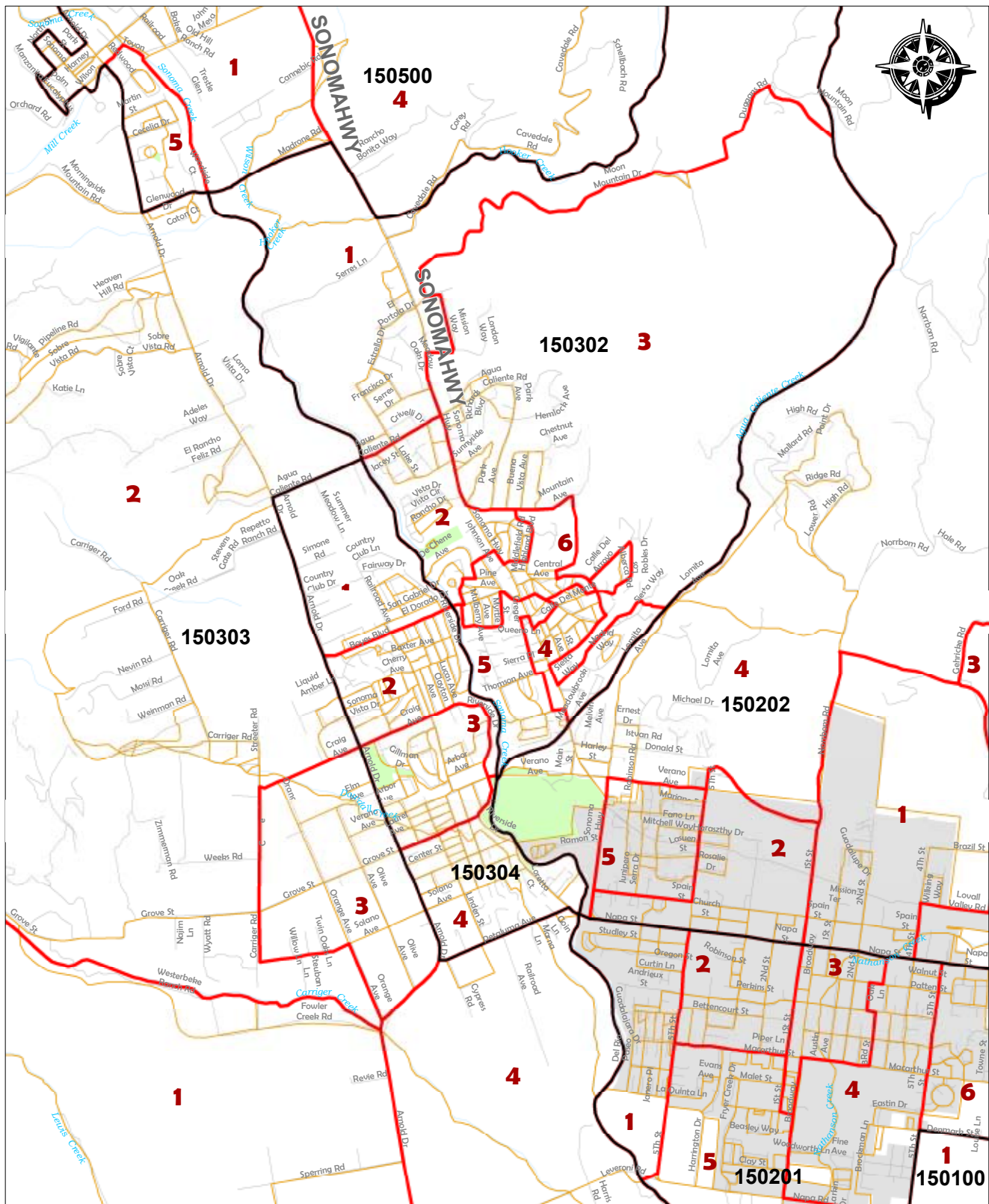
## DEMOGRAPHICS OF STUDY AREA

### Demographic Background

The Springs CBTP study area is comprised of eight Census Block Groups (CBGs), each of which has been assigned a discrete number by the U.S. Census Bureau. (Please see map on page 10). Each CBG number in the study area begins with either 06097150302 or 06097150304, therefore to streamline referencing them for this plan, only the last two distinct numbers will be used as follows:

- 060971503022 will be referenced as CBG 22
- 060971503024 will be referenced as CBG 24
- 060971503025 will be referenced as CBG 25
- 060971503026 will be referenced as CBG 26
- 060971503041 will be referenced as CBG 41
- 060971503042 will be referenced as CBG 42
- 060971503043 will be referenced as CBG 43
- 060971503044 will be referenced as CBG 44





The population of the MTC-designated "Community of Concern" is in one Census Tract, #150302. The 2000 Census lists Census Tract #150302 as having 9,227 people; which is the population used in MTC's Equity Analysis Transportation 2030 report. This tract is made up of six CBGs. MTC, however, gave SCTA flexibility in determining parameters of the study area. Following examination of area demographics; and field observations, SCTA set boundaries of the study area to include eight CBGs, including four out of six of the CBGs in Census Tract 150302. The most northern and most eastern CBGs of Census Tract 150302 were removed as these areas are generally more affluent, as well as more sparsely populated. Four CBGs of Census Tract 150304 were added to the study area. These areas are to the east of the original "Community of Concern," between Sonoma Creek and Arnold Drive. While median household incomes of these CBGs reflect their economically mixed nature, much low-income housing in The Springs and three of its public schools are located in these CBGs.

It should be noted that for purposes of presenting demographics, setting study area parameters is needed, however, residents of the whole Springs area were included in the community outreach process. Proposed transportation improvements arising from this plan may be expected to benefit the larger population and areas beyond the extent of the identified study area. The study area boundaries were set to focus the analysis, not to be exclusionary.

Upon examining median household income levels, it was noted that several areas south of the study area and what is known as The Springs, including one CBG within the city of Sonoma, have average incomes lower than parts of the study area. As stated above, residents of these areas may also benefit from proposals coming out of this plan.

Also notable is that Census 2010 is currently underway, thus Census 2000 statistics presented in this

plan are a decade old. It is anticipated that the Census update will provide a more accurate view of the economic recession of recent years.

### Census Data

Based on the 2000 Census, the total population of the study area is 12,265. The numbers of people, families, and workers found in the entire study area and each individual CBG are shown below.

	PEOPLE	FAMILIES	WORKERS
<b>Study Area</b>	<b>12,265</b>	<b>2,928</b>	<b>5,916</b>
CBG 22	2,078	522	909
CBG 24	1,239	307	614
CBG 25	1,601	342	811
CBG 26	1,521	305	612
CBG 41	875	237	398
CBG 42	1,688	396	843
CBG 43	1,757	441	1,056
CBG 44	1,506	378	673

Median household income was \$53,076 for Sonoma County as a whole, compared to \$48,310 for the entire study area according to the 2000 Census. The lowest average household income can be found in CBG 26 at \$31,563. A snapshot of study area economics is presented below. Poverty numbers represent household incomes under 200% of the federal poverty level (poverty levels are determined by a combination of income and number in a household). The first set is for the study area as a whole; with CBG statistics following:

	HOUSE HOLDS (HHS)	AVER AGE HH SIZE	MEDIAN HH INCOME	PER SONS IN POVERTY	% IN POVERTY
<b>Study Area</b>	<b>4,265</b>	<b>2.88</b>	<b>\$48,310</b>	<b>4,076</b>	<b>33%</b>
CBG 22	687	2.95	\$44,087	484	23%
CBG 24	550	2.66	\$42,981	487	39%
CBG 25	417	3.48	\$51,985	850	53%
CBG 26	423	3.36	\$31,563	833	55%
CBG 41	269	2.82	\$65,938	155	18%
CBG 42	670	2.44	\$51,635	325	19%
CBG 43	662	2.96	\$53,594	587	33%
CBG 44	587	2.52	\$44,696	355	24%



(Please see maps on pages 13 and 14, showing median household incomes and percentages of the population in poverty, respectively).

The following Census 2000 data shows study area and CBG racial summaries. The numbers of "Black," "American Indian," "Hawaiian/Pacific Islander," and "Other" ethnic groups had little to no representation in the study area.

	WHITE	LATINO	ASIAN	2 OR MORE	TOTAL NON-WHITE
<b>Study Area</b>	<b>7,276 (59%)</b>	<b>4,426 (36%)</b>	<b>126 (1%)</b>	<b>274 (2%)</b>	<b>4,989 (41%)</b>
CBG 22	1,300 (63%)	657 (32%)	20 (1%)	60 (3%)	778 (37%)
CBG 24	781 (63%)	394 (32%)	13 (1%)	39 (3%)	458 (37%)
CBG 25	618 (39%)	928 (58%)	16 (1%)	25 (2%)	938 (61%)
CBG 26	513 (34%)	958 (63%)	12 (1%)	28 (2%)	1,008 (66%)
CBG 41	688 (79%)	148 (17%)	17 (2%)	17 (2%)	187 (21%)
CBG 42	1156 (68%)	466 (28%)	13 (1%)	29 (2%)	532 (32%)
CBG 43	1193 (68%)	447 (25%)	24 (1%)	50 (3%)	564 (32%)
CBG 44	1027 (68%)	428 (28%)	11 (1%)	26 (2%)	479 (32%)

The majority of the Latino population is of Mexican heritage; and most speak at least some English. The Latino population, however, is not uniform in terms of, for example, income, length of residence in county or country, education, English language proficiency, birth country, legal status, nationality, or community involvement.

The percentage of Latinos in the study area is 36%, which is higher than the 17.3% for Sonoma County as a whole in 2000. As a county, the percentage of the population with Latino roots has risen from 4% in 1970, to 6.9% in 1980, to 10.6% in 1990, to 17.3% in 2000, to the current estimate for 2010 of 24.3%.

The 2009 Sonoma County Demographic Profile (Sonoma County Economic Board, 2009) reports that by about 2030 the

percentage of the population classified as "White" will be 50% in Sonoma County, dropping to 34% by 2050. The corresponding Latino percentage is forecast to be 50.7% by 2050—a nearly 200% increase from year 2000. The changing percentages will be largely due to natural increase and differences in birth rates by ethnicity. During the 2004-2006 period the number of births per 1,000 in the population was twenty-four for Latinos as compared to about eight for Whites. For young adults (aged 15-19 years) this rate per 1,000 females was about seventy-two for Sonoma County Latinos; about ten for Whites (2004-2006).

Forty-five percent of all children in Sonoma County under five years old are Latino (Source U.S. Census Bureau per article in The Press Democrat, 5/13/08, by Martin Espinoza). This figure was reported to represent a nearly forty percent increase over 2000 figures.

Focusing on transportation, it is useful to understand how many people are commuting and what their primary modes of transportation are. The list below provides data on workers who work at home, thus those who avoid the commute altogether.

Study Area: 207 (3%)

CBG 22: 78 (9%)

CBG 24: 15 (2%)

CBG 25: 0 (0%)

CBG 26: 3 (0%)

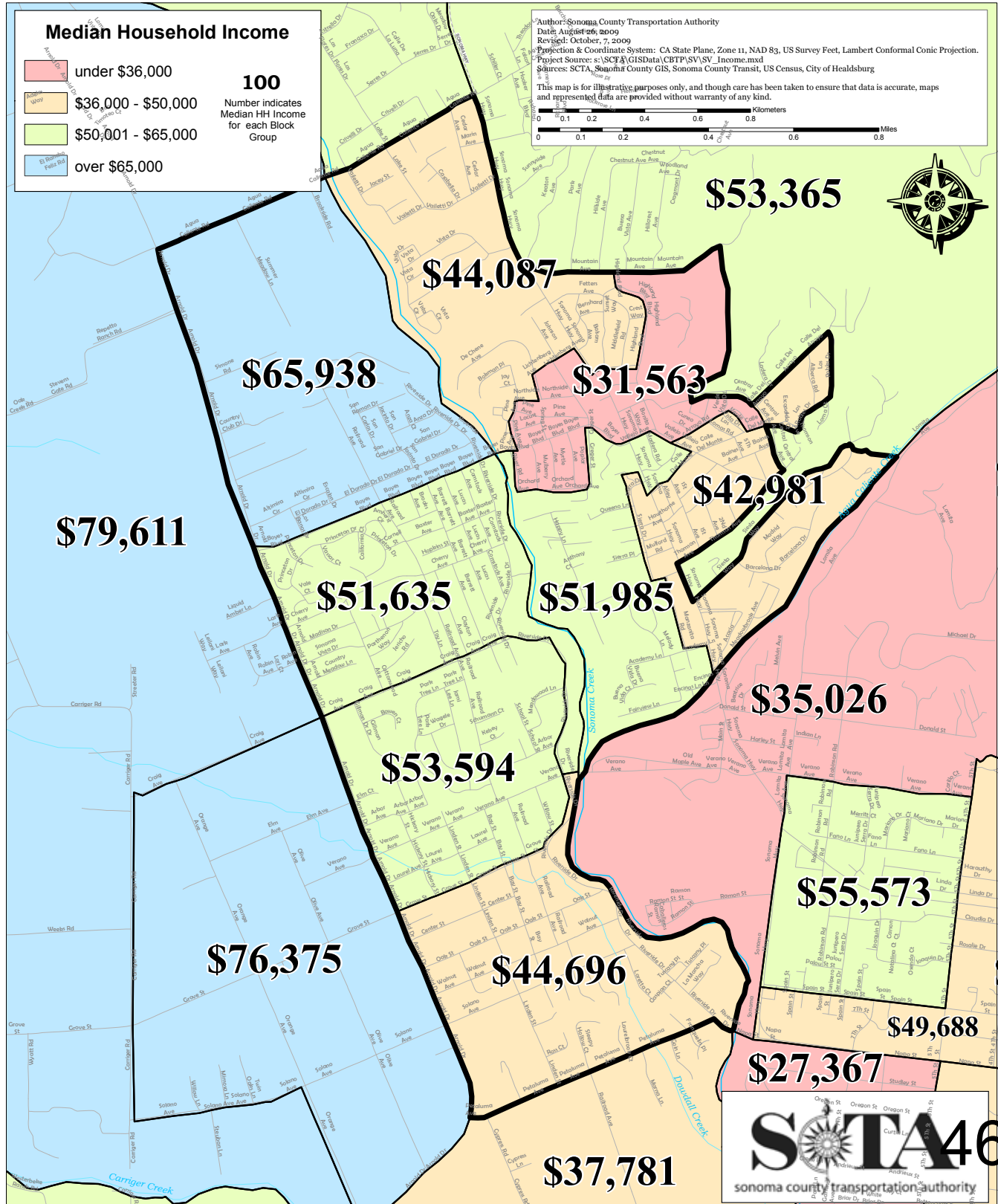
CBG 41: 16 (4%)

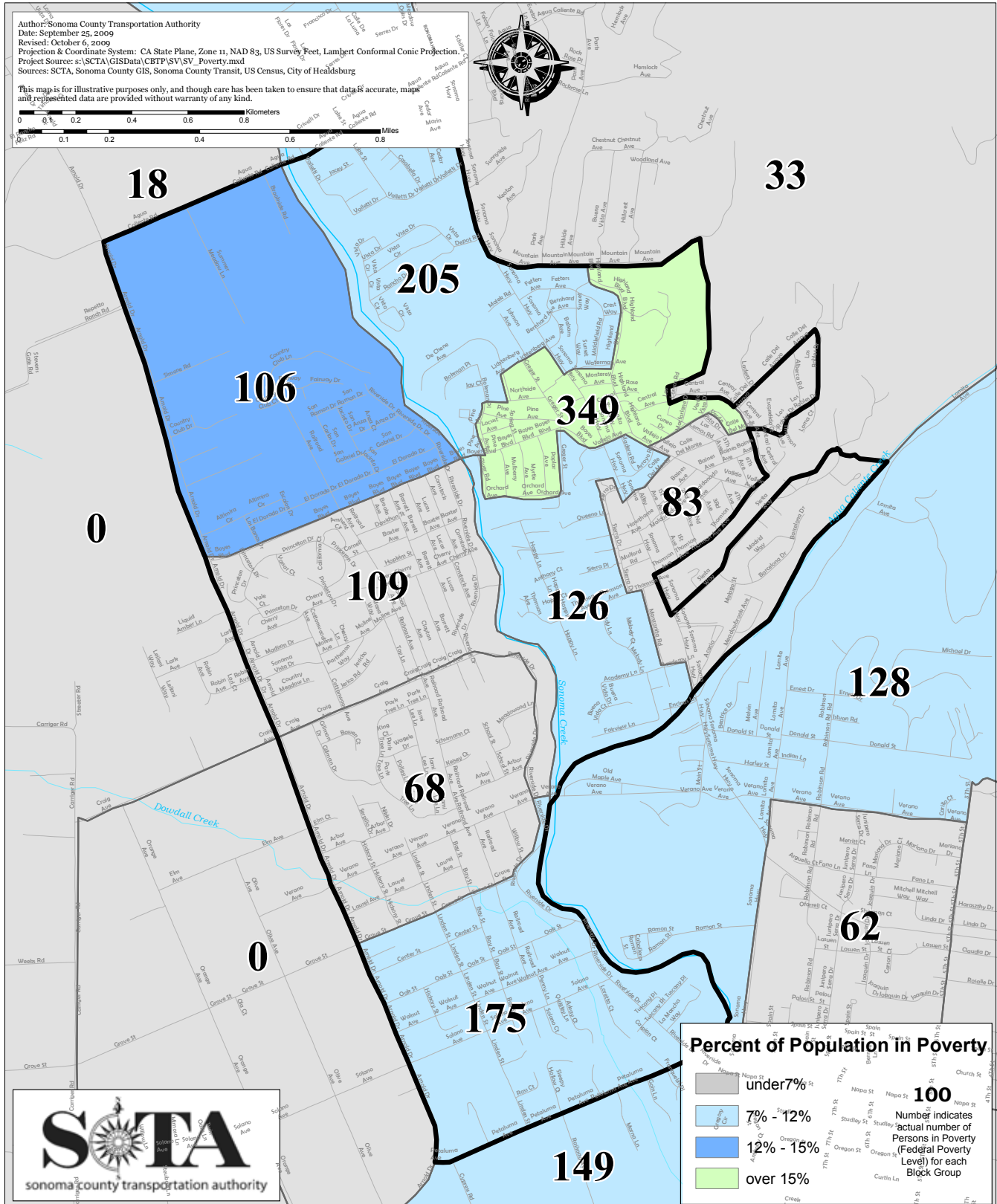
CBG 42: 46 (5%)

CBG 43: 25 (2%)

CBG 44: 24 (4%)

In the study area as a whole 88% of commuters (5,231) drove cars, however, in some of the CBGs the percentage of carpooling was high—as much as 19%. The following data shows the number of people and percentages of commuters they represent using various modes (2000 Census).





	DRIVE ALONE	CARPPOOL	TRANSIT	BIKE	WALK	MOTORCYCLE	OTHER
Study Area	4,364 (74%)	867 (15%)	119 (2%)	48 (1%)	228 (4%)	42 (1%)	41 (1%)
CBG 22	659 (72%)	126 (14%)	14 (2%)	21 (2%)	11(1%),	0 (0%)	0 (0%)
CBG 24	450 (73%)	79 (13%)	21 (3%)	0 (0%)	31 (5%),	7 (1%)	11 (2%)
CBG 25	539 (66%)	149 (18%)	25 (3%)	27 (3%)	71 (9%),	0 (0%)	0 (0%)
CBG 26	413 (67%)	102 (17%)	12 (2%)	0 (0%)	82 (13%),	0 (0%)	0 (0%)
CBG 41	348 (87%)	23 (6%)	9 (2%)	0 (0%)	0 (0%),	0 (0%)	2 (1%)
CBG 42	658 (78%)	103(13%)	13 (2%)	0 (0%)	18 (2%),	0 (0%)	0 (0%)
CBG 43	776 (73%)	201 (19%)	17 (2%)	0 (0%)	15 (1%)	22 (2%)	0 (0%)
CBG 44	521 (77%)	79 (12%)	8 (1%)	0 (0%)	0 (0%),	13 (2%)	28 (4%)

While the cited data on travel modes is useful, it should be noted that it is limited in scope because it pertains only to work trips. Trips for school, errands, medical or business appointments, childcare, recreation and shopping are not captured. The shortest leg of travel is also not captured. For example a person's walking or bicycling to a bus stop to continue their trip by bus for a greater distance, is not represented.

The percentage of people in the study area who drove alone to work per the 2000 Census was 74%—higher than the 68% for the Bay Area as a whole, and higher yet than the aggregated 59.8% of the "Communities of Concerns." Study area transit use was noted to be only 2%, which is lower than the 2.4% for Sonoma County as a whole. For the entire Bay Area transit use accounted for 9.7% and in the aggregated "Communities of Concern" 13%. Only four of the forty-four "Communities of Concern" had a lower transit percentage. The number walking to work in the study area (4%) was higher than for both Sonoma County as a whole (3.1%) and the Bay Area as a whole (3.2%). The rate of walking, however, varied widely among the CBGs—from 0% to 13%.

Outreach revealed that for those with the lowest incomes, for example some day laborers, the percentage of those walking and bicycling is significantly higher. Many with the lowest incomes cannot afford transit fares, and thus they walk, bicycle or arrange rides as their primary modes.

As learned through study outreach, carpooling and giving rides to others

within circles of family and friends was reported to be widely utilized. Informal networking to gain transportation served the needs of many lower-income seniors, laborers, and Latino family members in particular. Others who are car-less by choice or circumstance, have the options of fixed-route transit services, taxis, or possibly paratransit services.

Within the study area, most households reported having at least one vehicle, however, an average of 4% of households (189 households) had no vehicle. The following shows the number and percentage of "no-vehicle households" by CBG:

CBG 22: 6%

CBG 24: 6%

CBG 25: 3%

CBG 26: 12%

CBG 41: 0%

CBG 42: 1%

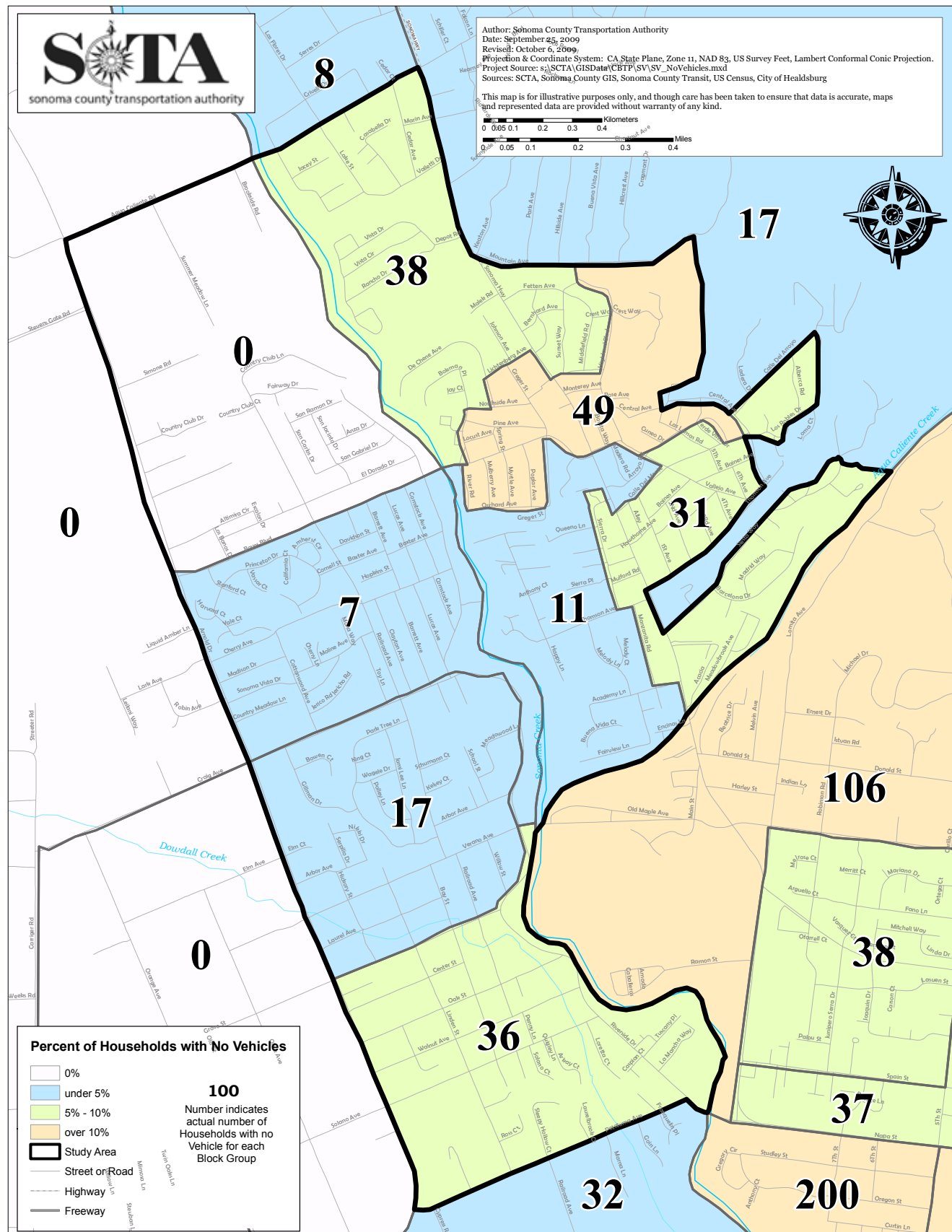
CBG 43: 3%

CBG 44: 6%

(Please see map on page 16 showing car-less households.)

The median age of Sonoma County's population is thirty-seven and a half years (in 2000). The numbers in two age groups are expected to significantly increase over the next eleven years. According to the Sonoma County Demographic Profile 2009, the population of people sixty-five years or older will rise by a projected 35,291 from 2007 to 2020. This group includes the bubble of the "boomer"





generation. Likewise, the number of young (aged minus one to twenty-four years) is expected to grow an additional 25,793 people from 2007 to 2020. Interestingly, and with significant ramifications for the County's workforce structure, only 1,170 additional people between the ages of twenty-five and sixty-four are projected during the same period.

Shown below are the numbers within each CBG of persons eighteen years and under; and persons sixty-five years or older.

	AGED 18 YEARS AND UNDER	AGED 65 YEARS AND OVER
<b>Study Area</b>	<b>3,482</b>	<b>1,016</b>
CBG 22	620	163
CBG 24	329	109
CBG 25	506	55
CBG 26	465	57
CBG 41	221	115
CBG 42	413	239
CBG 43	531	98
CBG 44	397	180

(The maps on pages 18 and 19, show the age distribution in the study area.) The age statistics are interesting as compared to the city of Sonoma. While 8.3% of the study area population is 65 years and over, in the city this percentage is 24.3%. In the study area 28.4% are 18 years and under, whereas in the city this figure is 18.5%.

### School Data

In addition to the Census, another interesting source of data is collected pertaining to students in the local schools. The State of California Department of Education requires schools to provide School Accountability Report Cards. Extrapolated from these reports, the following shows the number of students enrolled and the percentages of students in school years 04/05 and 07/08 of Latino and White students, and English Learners (EL) at each of the Sonoma Valley Unified School District's regular public schools in the study area.

#### **Altimira Middle School with 467 students in grades 6-8 in school year 07/08:**

04-05 Latino 40%, White 57%, EL 26%

07-08 Latino 58%, White 39%, EL 35%

#### **El Verano School with 418 students in grades K-5 in school year 07/08:**

04-05 Latino 69%, White 25%, EL 62%

07-08 Latino 76%, White 22%, EL 71%

#### **Flowery School with 371 students in grades K-5 in school year 07/08:**

04-05 Latino 72%, White 27%, EL 67%

07-08 Latino 77%, White 22%, EL 70%

The following compares the percentages of socio-economically disadvantaged students, based on their qualification for reduced-priced lunches, which is determined by family annual incomes. In 2007/2008 this was an annual income of \$38,206 for a family of four.

Data is for the school years 04/05 and 06/07 (data was not provided more recently).

#### **Altimira Middle School**

04-05: 39%

06-07: 53%

#### **El Verano School**

04-05: 68%

06-07: 71%

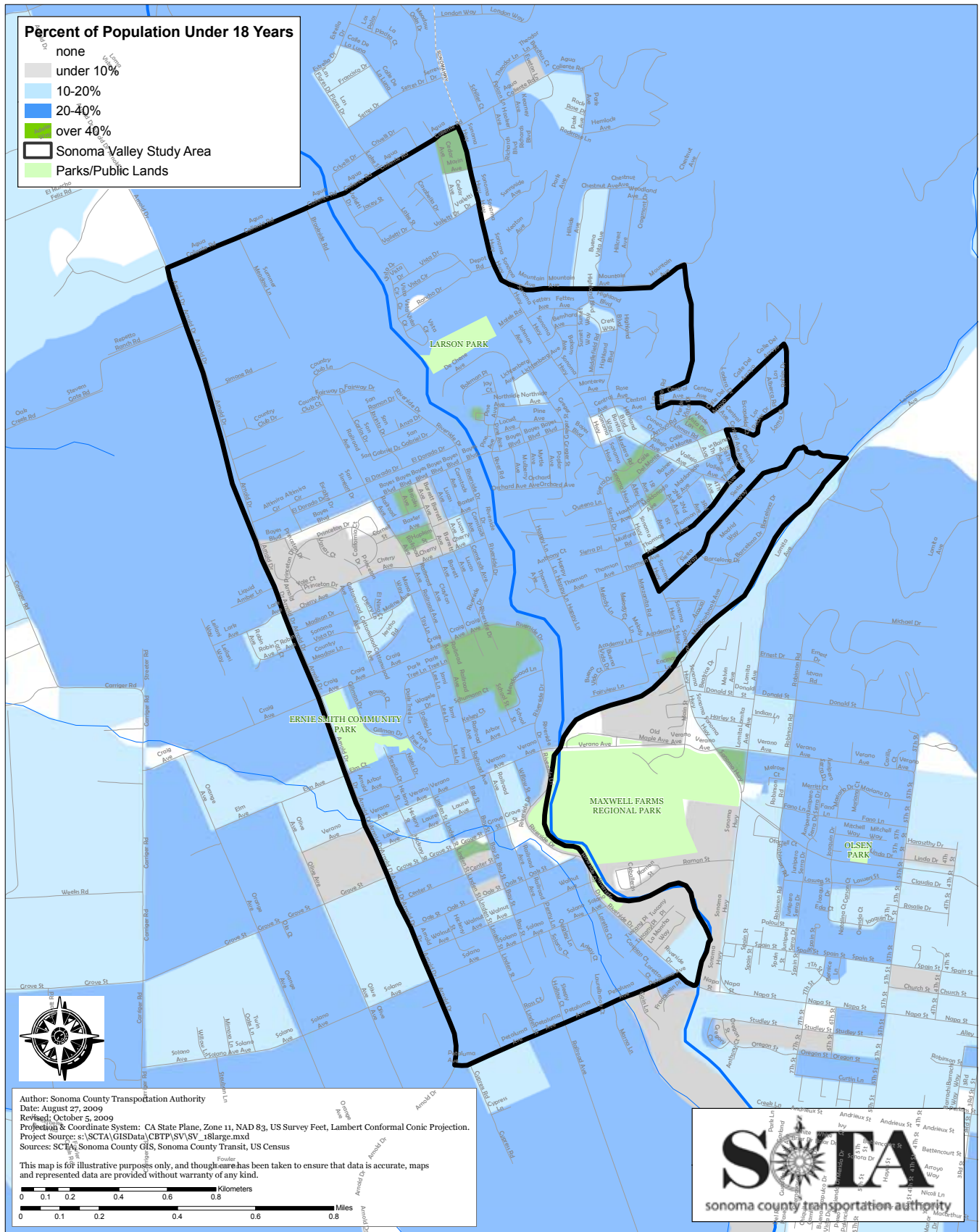
#### **Flowery School**

04-05: 81%

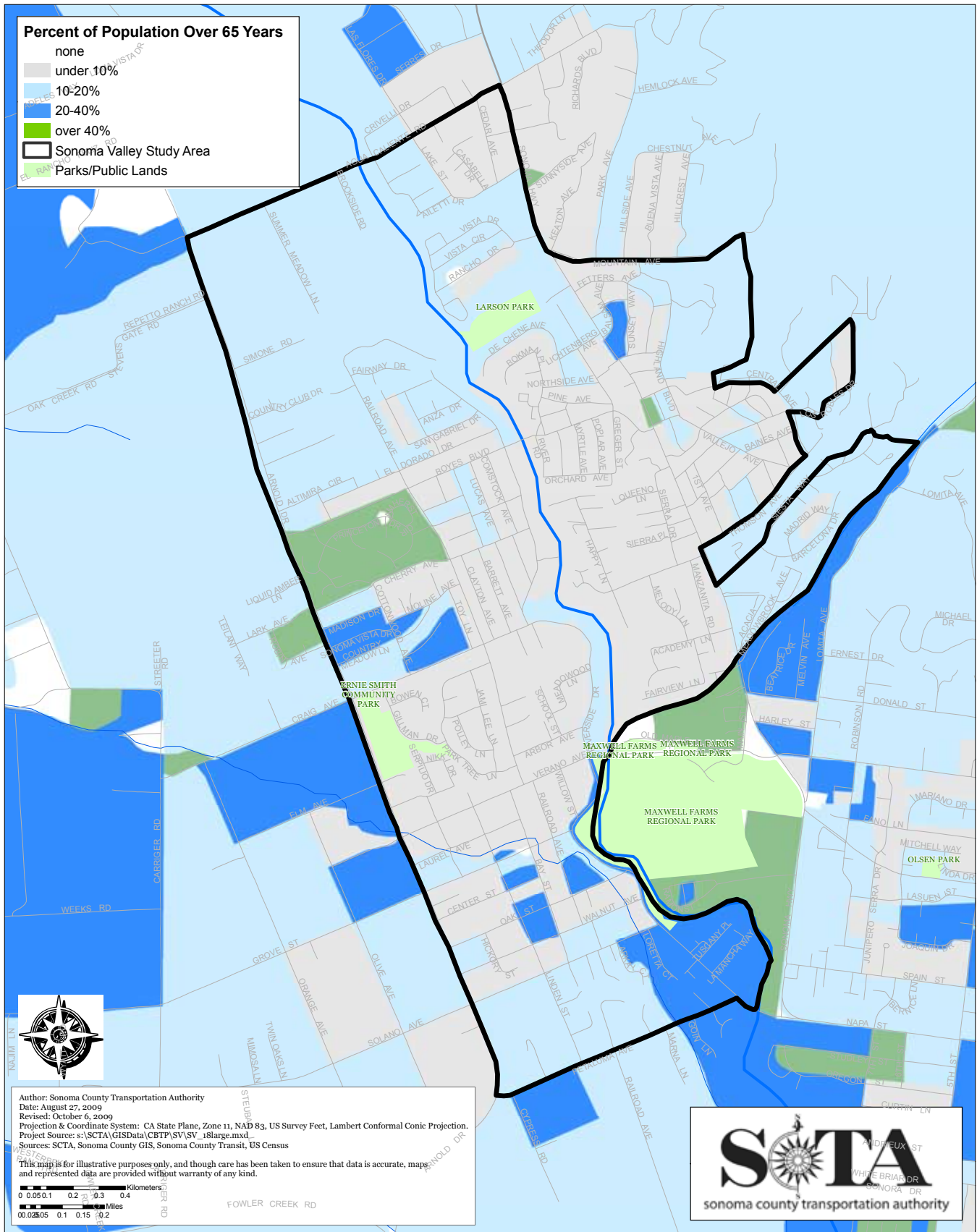
06-07: 78%

The public high school that serves most study area students in grades 9-12 is outside of the study area in the city of Sonoma. Enrollment there in 07/08 was 1470 students, 11% reported as English Learners; 60% as White; and 36% Latino/Hispanic. In 06/07, 26% were indicated as socio-economically disadvantaged.

While these statistics are "snapshots" in time, it is of interest to note that per the data, the public schools witnessed







increases from school year 04/05 to 07/08 in the percentages of Latino students and those who were designated as English language learners.

The drop-out rate is significantly higher for Latino youth than for White youth. In Sonoma County the rate is 22% for Latino females and 31.8% for Latino males (The Press Democrat, Robert Digitale, May 13, 2009). From the perspective of studying “Communities of Concern” it is clear that for the well being of both the community and individuals, there is compelling need to ensure that these children have adequate access to opportunities for education, social services and health care. Measures to foster full utilization of positive opportunities are of crucial importance.

### Growth Projections

The Association of Bay Area Governments (ABAG) is the comprehensive planning agency of the nine-county region and is responsible for forecasting population and employment growth for the San Francisco Bay region. Current ABAG forecasts for the study area indicate a very gradual rise in population, households, housing, hotel rooms, commercial space, and office space. The greatest increase is in square footage of office space, however, the rate of increase is only six percent from 2005 to 2035. The number of acres dedicated to recreational uses is predicted to remain unchanged by 2035. Square footage of industrial uses and number of students are predicted to be reduced (minus 0.75% for industrial and minus 14.29% for students).

### Homelessness Data

A homeless census was conducted in January 2009. This count identified 3,247 homeless people in Sonoma County (compared to 1,974 in 2007). Less than one percent of the homeless population was identified as residing in Sonoma Valley, which includes the city of Sonoma.

During the wine crush season, however, migrant farm workers come to the area to work. For the past six years Vineyard

Workers Services (now merged with La Luz) has provided temporary housing to accommodate these workers. In years past, two camps have been established. In 2009, due to funding shortfalls, only one site was provided and soon filled to capacity. The camp, located at Saint Leo's Catholic Church in Agua Caliente, housed sixty men. By mid-September, thirty-two workers had been turned away for lack of space (Sonoma Index Tribune. Emily Charrier-Botts, September 11, 2009), thus there may be seasonal rises in homelessness in the area.

## DESTINATIONS

### The Sonoma County Travel Model 2007

The model was examined to obtain a “snapshot” of study area trips (by all modes). While models do not provide precise measures, the results are interesting. Using rounded percentages, the first set is for all trips, second for work trips, and third for school trips (all ages). This data pertains to residents of all income brackets, not just those with lower incomes.

#### All Study Area Daily Person Trips:

- 29% were internal to the study area
- 28% were to other rural areas in Sonoma County outside the study area
- 21% were to the city of Sonoma
- 6% were to go south on Highway 101
- 5% were to Santa Rosa
- 5% were to Petaluma
- 4% were to Napa and Solano counties
- 1% were to Rohnert Park
- Trips to Cloverdale, Healdsburg, Windsor, Sebastopol, Cotati, and north on 101 were all less than 1%

#### Daily Person Work Trips:

- 18% were to go south on Highway 101
- 17% were to Santa Rosa

- 16% were to Petaluma
- 15% were to other rural areas in Sonoma County outside the study area
- 12% were to the city of Sonoma
- 9% were to Napa and Solano counties
- 5% were internal to the study area
- 4% were to Rohnert Park
- Trips to Cloverdale, Healdsburg, Windsor, Sebastopol, Cotati, and north on 101 were all less than 1%

#### **Daily Person School Trips:**

- 33% were internal to the study area
- 23% were to other rural areas in Sonoma County outside the study area (which includes the Sonoma State University campus)
- 17% were to the city of Sonoma
- 14% were to go south on Highway 101
- 7% were to Napa and Solano counties
- 3% were to Petaluma
- 1.5% were to Santa Rosa
- Trips to Cloverdale, Healdsburg, Windsor, Sebastopol, Rohnert Park, Cotati, and north on 101 were all less than 1%.

Significant for purposes of examining transportation needs is that for all trips, as well as for school trips, over 50% are to locations within the Sonoma Valley; however, for work trips the picture is quite different. Only five percent of work trips are internal to the study area. The largest group by percentage are workers who travel south on Highway 101 (18%), followed closely by those going to Santa Rosa (17%), and then to Petaluma (16%).

#### **Employment**

Percentages are not known, however, outreach revealed that most of the lower-income study area workers are employed within the study area at hotels, spas, stores, restaurants, and schools; outside the study area in the surrounding

vineyards and wineries, at the Sonoma Developmental Center in Glen Ellen, the Sonoma Valley Hospital in Sonoma, Infineon Raceway, Hanna Boys Center, and at local tourist-oriented businesses; as well as in the greater region including in the cities of Santa Rosa and Petaluma, and in Napa, Marin and Solano counties for construction, health care and service sector jobs. The largest private sector employer within the study area is the Fairmont Sonoma Mission Inn and Spa.

#### **Urban Services**

Almost all core services are available in Sonoma Valley, but not necessarily in the study area. The city of Sonoma has a hospital and health center, as well as the larger food and drug stores; a regional library; senior center, public high school; and medical, dental and professional services. The study area has two post offices—one in El Verano and one in Boyes Hot Springs. The businesses and offices of the study area are concentrated along Highway 12. Most of the stores and restaurants are small privately-owned businesses, most of which serve local residents. Examples include: taquerias; a paint store; auto repair shops; rug store; coffee shop; produce market; caterer; florist; food, meat and liquor stores; restaurants; party supply; glass shop; upholstery; hair salons; income tax preparation; money order store; veterinarian; and furniture stores. A small skilled nursing facility is located proximate Highway 12 (with nine residents/four employees); A larger facility is the Aqua Caliente Villa Home on Vailletti Drive with forty-five beds and about thirteen employees. There are several gas stations and fast food outlets in the study area. Two small newer shopping centers are located off Highway 12; one at the south end of the study area and one in the central area that has a bank and cinemas complex. Additionally, there are many single businesses and various small business complexes along Highway 12. The largest business is the Fairmont Sonoma Mission Inn and Spa that includes the historic Big 3 restaurant. They

employ approximately 500 employees; 330-350 of whom work full-time.

Residents must travel to Santa Rosa, the county's largest city, to access certain services, including governmental offices, and courts, as well as its major medical centers (e.g., Memorial, Kaiser and Sutter hospitals and medical centers). Mobile health and dental services are offered through St. Josephs in an attempt to bring these services into the study area. These by their nature, however, are available intermittently and are not sufficient for meeting the volume and range of community needs. A health clinic has been proposed at a site on Highway 12 in the study area, with partial funding secured. An additional \$4-6 million is needed. Patients currently access care at the Sonoma Valley Health Clinic in Sonoma. That facility serves 7,000 clients with 20,000 to 24,000 annual visits. Many of these clients travel from the study area for medical care.

The more urbanized areas are also destinations for employment for many study area residents. Most government, medical, retail, technical, service, agricultural, and construction jobs are only available outside the study area.

### **Schools and Buses**

There are various academic options in the study area. Altimira Middle School had 467 students in grades 6-8 in school year 07/08. The adjacent Woodland Star (Waldorf) Charter School has about 200 kindergarten through sixth grade (K-6) students. El Verano School had 418 students in grades K-5 in school year 07/08; and Flowery School had 371 students in grades K-5 in school year 07/08. The Montessori-based Sonoma Charter School has about 230 K-8 students. Many students who live in the study area attend schools outside the study area. This includes all high school students, most of whom attend Sonoma Valley High School on Broadway in Sonoma, as well as those students who attend schools in Sonoma and Glen Ellen (e.g., Dunbar and Sassarini), and more

distant schools such as Cardinal Newman/ Ursuline High School in Santa Rosa.

The Sonoma Valley Unified School District has a policy to provide bus service designed to transport students who would otherwise have to cross high volume streets and/or travel along unsafe routes. Rather than setting a distance threshold, the pick-ups are determined on a case by case evaluation of need. The difficulty of finding safe places to pull off roads for pick-ups and drop-offs of students, as well as places to turn around makes provision of bus service problematic. Arnold Drive in particular is a roadway that has significant traffic volumes, but few safe drop-off/pick-up places that do not further delay traffic. The school district provides regular school buses and First Student provides bus transportation for special needs students. Due to potential additional State of California budget cuts, it is unknown at present how Sonoma Valley Unified School District services will be impacted. Some districts have eliminated or curtailed non-mandatory busing.

### **Adult Education**

Adults seeking academic offerings must travel outside the study area for schooling, or access distance learning from home. College/university sites closest to The Springs include Santa Rosa Junior College (SRJC), Empire Business College and Law School, and University of San Francisco (North Bay Regional Campus) in Santa Rosa; Dominican University in San Rafael; Napa Valley College in Napa, and Rohnert Park's Sonoma State University (SSU). SRJC also has a campus in Petaluma. SRJC offers limited off-campus classes. Several of these classes are held in Sonoma Valley at the high school and senior center in Sonoma, and a few at the Sonoma Developmental Center (SDC) in Eldridge. Sonoma State University is also offering several Osher Lifelong Learning Institute classes at the senior center in Sonoma.

It is notable that there is no direct bus service from Sonoma Valley to Napa or Napa Valley College.



### Social/Senior Services

La Luz is an active non-profit organization that is located in The Springs. La Luz helps the mostly Latino and immigrant community by providing assistance with education, enrichment, employment, counseling, nutrition assistance, and health and social services. In June 2009 the Vineyard Worker Services (VWS) organization was merged with La Luz. VWS provides social services, education, and housing assistance to migrant workers.

The Valley of the Moon Teen Center (VOMTC) is in the study area, sited on Highway 12. The center is a non-profit organization that serves young people thirteen through eighteen years old. Offerings include programs focused on health, nutrition, exercise, job readiness, music, gang prevention, and personal development.

Nuestra Voz is another non-profit, grassroots organization offering community-based programs of exercise, recreation, and education. All age groups are served.

Sonoma Valley Mentoring Alliance, in existence for more than twelve years, is a non-profit organization that serves Sonoma Valley youth by facilitating volunteer mentoring, organizing enrichment opportunities, and supporting mentors and mentees.

The Vintage House Senior Center is an activity and resource center located in the city of Sonoma. The center offers an array of opportunities for exercise, education, nourishment, well-being and enrichment.

Friends in Sonoma Helping, known as FISH, is a long-established program that has been providing rides for primarily medical appointments. FISH also distributes food and bus vouchers to those in need.

### Veterans' Services

Some services for veterans are available locally in Sonoma County; others are available only in San Francisco. The Sonoma County Veterans Service Office

in Santa Rosa provides assistance to veterans and dependents with Veterans Affairs (VA) benefits. In October 2009, the expanded United States Department of Veterans Affairs Outpatient Clinic opened near the county airport in northwest Santa Rosa when its predecessor on Chanate Road closed. Services provided there include primary care, specialty care, and mental health. The San Francisco Veterans Medical Center at Fort Miley provides medical, surgical and psychiatric services. A free shuttle is available from Santa Rosa to Fort Miley.

### Recreation/Trails

In addition to the recreational programs offered through the local schools, Maxwell Farms Regional Park is an eighty-five acre facility that provides sports fields, a children's playground, skateboard park, picnic sites, and forty acres of nature trails. This site adjacent the southern end of the study area is also home of the Valley of the Moon Boys and Girls Club. The club is housed in a 23,000 square foot multi-use facility offering sports, computer training, tutoring and youth art classes for children residing in the entire Sonoma Valley. A school bus takes children from all area schools to the club.

Larson Regional Park is also one of Sonoma County's regional parks and is located on DeChene Avenue in Boyes Hot Springs, west of Flowery School. In addition to tennis courts, multi-use and softball fields, a children's playground area, and picnic area, the facility provides access to riparian habitat along Sonoma Creek. A community garden is to be initiated there soon.



In El Verano, Sonoma County Regional Park's Ernie Smith Park has sports fields, a playground, dog park, picnic areas including those accessible by wheelchair, and paved multi-use trails. The park parcel is irregular in shape extending eastward from Arnold Drive between Craig Avenue and Elm Court all the way to Railroad Avenue.

(Please see map on page 25, showing study area destinations, including parks, schools and bus stops, as well as bicycle facilities.)

Sonoma County Regional Parks is in design phase of the Central Sonoma Valley Trail. They are also negotiating trail easements from the Sonoma Valley Unified School District. One trail easement would be located on the Sonoma Charter School site and the second trail easement would be located on Flowery School. Construction of the charter school trail segment is anticipated in spring 2010.

The privately-owned Agua Caliente Springs Aquatic Center is also located in the study area on Vailetti Drive. This year-round swim center serves all ages. Approximately 300 members, predominantly Central Sonoma Valley residents, use the facility each day. Fifteen full or part-time people are currently employed there.

### **Food Sources**

The Community Activity and Nutrition Coalition of Sonoma County (CAN-C) and the Sonoma County Department of Health Services (DHS), working with the statewide Communities of Excellence in Nutrition, Physical Activity, and Obesity (CX3) program studied four low-income neighborhoods in Sonoma County, one of which was Boyes Hot Springs. This study area had a population of 9,227, fifty-five percent of which were Hispanics living in poverty. While smaller than the CBTP study area, comprised of roughly the eastern part, the findings are relevant to the CBTP area. The area was researched for food access and found to have no supermarkets or farmers markets; only six small markets and two convenience stores.

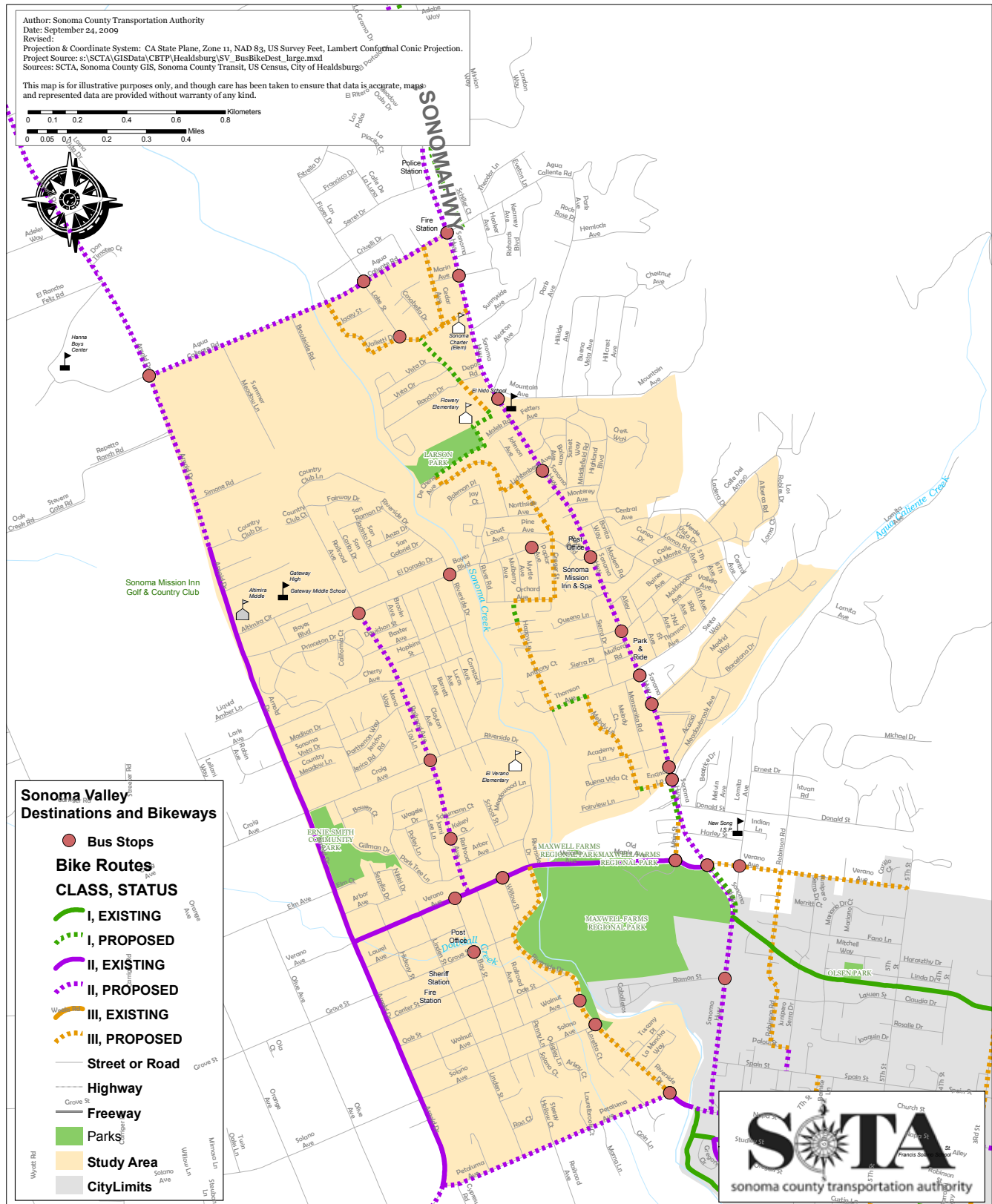
The gathered data allows a greater understanding of the dynamics shaping health behaviors. The study area had no food stores that met the "quality standards" for accessible, healthful and affordable food. For those who are dependent on local food sources due to transportation limitations, the ramifications can be manifested in negative health outcomes, such as obesity. In addition to increasing the number of grocery stores offering healthy foods, recommended actions included expansion of public transit, and development of pedestrian and bicycle facilities to improve access to healthy foods.

### **LAND USES & PROPOSED DEVELOPMENT**

Overall, the greatest use of land in the study area is designated "urban residential." There is also some "rural residential," however this is generally farther out from the study area. Other uses include "public and quasi public" land where schools, parks, and government buildings are located. Additionally there are a few areas designated as "general commercial" mostly proximate the Highway 12 corridor. In the northwest quadrant of the study area, there remain sizable tracts of agricultural land. The study area has several housing complexes designated as "affordable." Included are The Springs Village with eighty "all age" units on Vailetti Drive, and the Finish American Housing Association's F.A.H.A. Manor on Verano Avenue with forty-eight units for people who are over 62 and/or have disabilities. There are also several mobile home complexes in the southeastern quadrant of the study area. One is a senior housing park adjacent Highway 12 near Agua Caliente Creek.

Only one major development project is pending in The Springs area: A fifty-two-home subdivision at 17310 Highway 12. This project has been approved, but is not yet under construction.

Sonoma County has recently updated its General Plan. Policies regarding growth and development have thus been recently







discussed and decided. The rate of growth in the study area is anticipated to be slow.

## TRANSPORTATION INFRASTRUCTURE & CONDITIONS

### Roads

There are arterial, collector, and local streets in the study area. Arterials carry the most traffic, then collectors, then local streets. State Highway 12 (Sonoma Highway) is the only Urban Principal Arterial. It is an inter-regional roadway, operated by the California Department of Transportation (Caltrans). Three study area roads are classified as Urban Minor Arterials: Aqua Caliente Road, Arnold Drive and Verano Avenue. Three are classified as Urban Major Collectors: Boyes Boulevard, Railroad Avenue and Petaluma Avenue. Two are classified as Urban Minor Collectors: Craig Avenue and Riverside Drive.

The recently adopted Sonoma County General Plan 2020 indicates that four study area roadways are planned to be expanded to three lanes of travel, however, there are no active plans to fund, design or construct these facilities. These are: Aqua Caliente Road, Arnold Drive, Verano Avenue and Petaluma Avenue. Currently all of these roadways are two-lane roads.

Planned and completed Highway 12 improvements include sidewalks in compli-

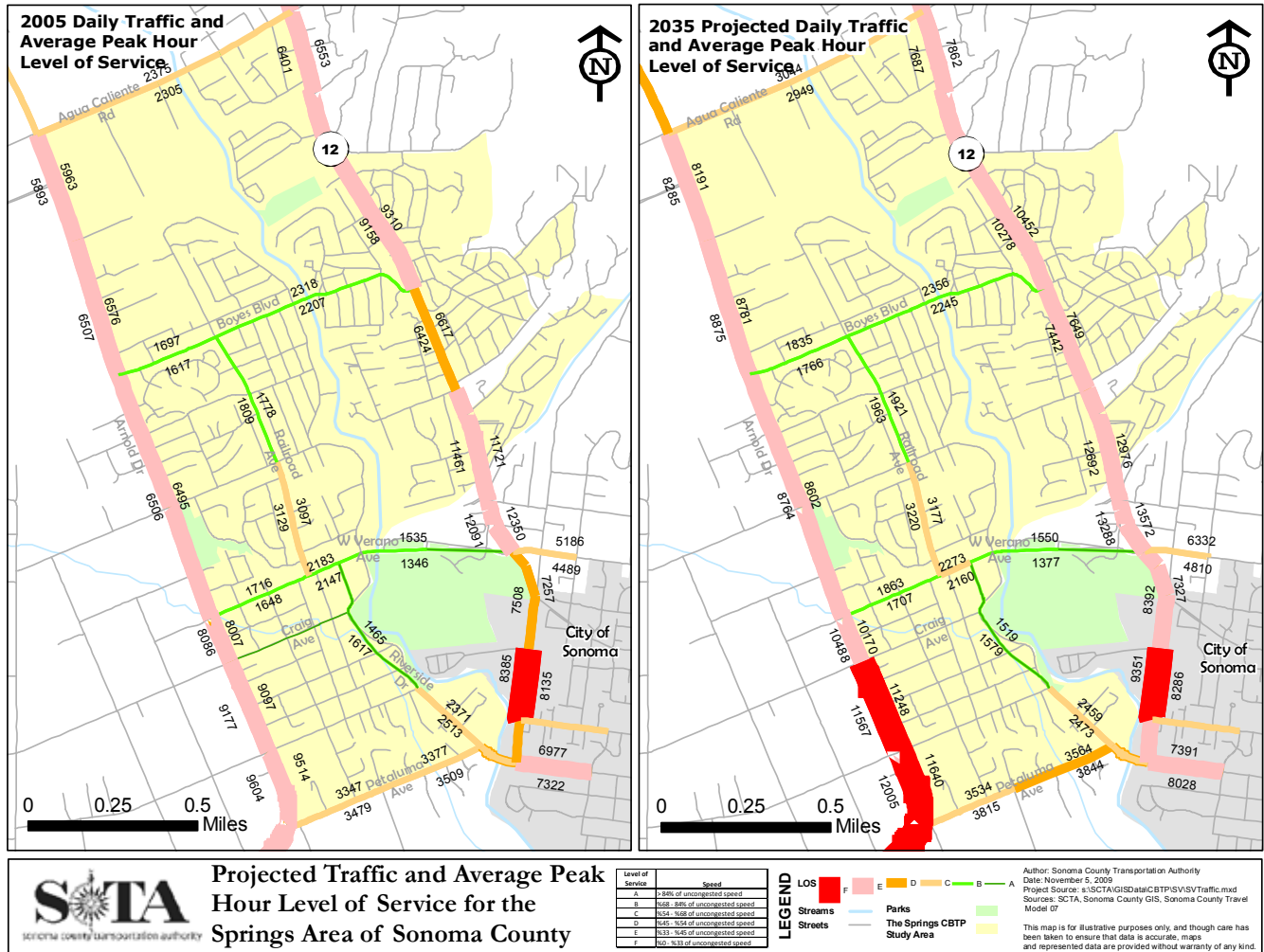
ance with Americans with Disabilities Act (ADA) standards, ornamental street lighting, enhanced storm drainage treatment facilities, curbs, gutters, and other pedestrian enhancements. The first stretch of the planned Highway 12 widening and sidewalk project is complete. While Highway 12 is one of the County's most congested roadways, further widening is impossible due to the limited right-of-way. This recently completed project runs from Donald Street (just north of Verano Avenue) north to Boyes Boulevard. Eight foot minimum shoulders and sidewalks have been constructed. A second phase of the widening and sidewalk project will continue north from Boyes Boulevard to Agua Caliente Road and will also include eight foot minimum shoulders and sidewalks. This project is not anticipated to be constructed for several years. Pre-construction work is currently underway to relocate utilities underground and acquire necessary rights-of-way. There is also a gap in the sidewalk on the east side of Highway 12 between Verano Avenue and Donald Street. This sidewalk construction may be included in a future construction stage.

### Traffic

The maps on page 27 compare traffic volumes in 2005 and 2035. Numbers show the number of vehicles per day per direction of travel. As can be seen, traffic volumes are projected to increase on several study area roads. The two roads with the greatest traffic congestion are not surprisingly Arnold Drive and Highway 12, with the highest level of congestion predicted to be on Arnold Drive south of Craig Avenue.

### Creeks & Bridges

The Springs has several creeks running through it. Sonoma Creek is the largest and runs north to south through the entire study area. This creek creates a natural barrier for mobility in that there are only three road crossings in the study area: Aqua Caliente Road crosses the creek at the north end; Boyes Boulevard in the middle; and Verano Avenue to the



south. The Sonoma County Transportation and Public Works Department is planning to replace the bridge on Boyes Boulevard. In the Riverside Drive/Grove Street area a segment of road adjacent the creek has collapsed. A short detour skirts the closure and there are no plans to rebuild that road segment. The Pequeno Creek is located in the mid-eastern part of the study area; Aqua Caliente Creek in the southeastern part; Dowdall Creek in the southwest part. The Highway 12 bridge over Aqua Caliente Creek presents a width constraint to adding bicycle/pedestrian facilities along that segment of the roadway.

### Bicyclist and Pedestrian Facilities

Class I facilities are separated from roadways (such as the Joe Rodota Trail);

Class II facilities are on-road bicycle lanes designated with striping and signage and/or pavement markings; and Class III facilities are on-road, "share the road," bicycle routes indicated just with signage. Unpaved recreational trails are another form of pedestrian/bicycle facility.

Within the study area, existing Class II bicycle lane facilities are located along Arnold Drive between Petaluma Avenue and Country Club Drive, along Verano Avenue between Arnold Drive and Sonoma Creek, and along Riverside Drive between Petaluma Avenue and Highway 12. A Class I bicycle and pedestrian pathway is located between Railroad Avenue and Craig Avenue running through Ernie Smith Park.

There are also several bicycle facilities proposed for the study area in the



Sonoma County Bikeways Plan including planned Class II bicycle lanes along Arnold Drive between Country Club Drive and Agua Caliente Road, along Agua Caliente Road between Arnold Drive and Highway 12, along Railroad Avenue between Verano Avenue and Boyes Boulevard, and along Petaluma Avenue between Arnold Drive and Riverside Drive.

Class III bicycle routes are proposed in the Bikeways Plan within the study area along Riverside Drive between Petaluma Avenue and Verano Avenue, along Highway 12 between Agua Caliente Creek and Agua Caliente Road, and along various residential streets that are identified as segments of the Central Sonoma Valley Trail to the west of Highway 12 between Verano Avenue and Agua Caliente Road.



The proposed Central Sonoma Valley Trail project includes several Class I pathway segments proposed as bicycle and pedestrian connections between the various Class III bicycle route segments. These Class I pathway connections are proposed between Encinas Lane and Fairview Lane, between Happy Lane and Orchard Avenue, between Larson Regional Park and Flowery Elementary School, and between Depot Road and Valetti Drive. (Please see map on page 25, showing bicycle facilities.)

Other than the proposed Class I pathways mentioned above for the Central Sonoma Valley Trail project, there are few other pedestrian facilities currently planned within The Springs study area. The County's redevelopment agency recently completed the construction of continuous new sidewalks along Highway 12 between Agua Caliente Creek and Boyes Boulevard. To the north, between Boyes Boulevard and Agua Caliente Road along Highway 12, sidewalks become intermittent and disconnected. However, long-term plans by the County's redevelopment agency propose new continuous sidewalks along this segment of Highway 12. Beyond the Highway 12 corridor, Boyes Boulevard between Railroad Avenue and Greger Street includes a pedestrian walkway along the southern portion of the road that is separated by a raised asphalt berm. Many residential streets located within the study area include standard "curb and gutter" pedestrian sidewalks. Pedestrian pathways are also provided throughout Ernie Smith Park.

### Bicycle Safety

In-the-field observation indicated that a number of bicyclists of all ages are not using best bicycle safety practices. Behaviors not being practiced include helmet use, using reflective and/or light colored clothing at night, understanding the rules of the road, and riding with traffic. Many Latino immigrants rely on bicycles and walking as their primary means of transportation, however, many are not familiar with local traffic signs, signals, and practices. Additionally,



language barriers may further impact the ability to understand how to safely travel. Latinos, and studies have indicated Latino adult men in particular, are at a higher risk of being involved in pedestrian and bicycle crashes.

Educational outreach programs could increase the number of people who adopt safe practices.

## PUBLIC TRANSIT SERVICES

### Sonoma County Transit Services

Public transit service in The Springs is provided by Sonoma County Transit (SCT). SCT's fixed-route system provides countywide service along major travel corridors in rural areas of Sonoma County. The system also links most small towns and communities and all nine incorporated cities in the County including Cloverdale, Healdsburg, Windsor, Santa Rosa, Sebastopol, Rohnert Park, Cotati, Sonoma and Petaluma. SCT operates twenty routes Monday through Friday between 5:00 a.m. and 11:00 p.m. Weekend service consists of thirteen routes operating on Saturday and nine on Sunday between 7:00 a.m. and 9:00 p.m. SCT's major intercity routes consist of routes 20, 26, 30, 40, 44, 48 and 60. Express and commute bus service is also provided via routes 22, 34, 38, 42, 46 and 62.

In addition to intercity public transit service, SCT provides local public transit service, under contract, within the Town of Windsor (route 66), and the cities of Sebastopol (route 24), Rohnert Park and Cotati (routes 10, 12, 14) and Sonoma (route 32), respectively. Local service is also provided within the unincorporated Lower Russian River area (route 28) and unincorporated Sonoma Springs communities (route 32). Weekend intercity service is also provided from July through September to the unincorporated Sonoma Coast communities of Freestone, Bodega, Bodega Bay, Jenner, and to the unincorporated Lower Russian River area (route 29).

Intercity route 30, local route 32, express route 34, and commuter route 38 all

serve The Springs. Route 30 operates daily providing regular and express service between Santa Rosa and the study area. The regular route serves the County Administration Center (weekdays) or Coddington Mall (weekends), Santa Rosa Junior College, Santa Rosa Transit Mall, Montgomery Village and Oakmont in Santa Rosa. It then travels to the Central Sonoma Valley via Kenwood, Glen Ellen and the Sonoma Developmental Center. Route 30 also serves a park-and-ride lot located in Boyes Hot Springs on Highway 12 near Thomson Avenue.

Similar to the regular route 30, route 30 express travels between Santa Rosa and the study area via Highway 12, but bypasses the community of Glen Ellen, Sonoma Developmental Center, and El Verano. Most of route 30's regular and express service interlines with route 20 providing overlapping service between the Santa Rosa Transit Mall and either the County Administration Center (weekdays) or Coddington Mall (weekends) in Santa Rosa.

Route 32 provides local service weekdays and Saturdays within the city of Sonoma and between the communities of Agua Caliente, Boyes Hot Springs and El Verano, as well as the Temelec senior community. Route 32 provides service to most major shopping centers within the city of Sonoma, the Fairmont Sonoma Mission Inn and Spa, Fiesta Plaza Shopping Center, Maxwell Farms Regional Park, Sonoma Valley Hospital, Sonoma Medical Plaza, Sonoma Valley High School, Sonoma Plaza, Vintage House Senior Center, and several mobile home parks in the area.

Route 34 provides one morning express commute trip and one evening express commute trip between the Santa Rosa Transit Mall in Santa Rosa, Kenwood, the communities of Agua Caliente, Boyes Hot Springs, El Verano, and the city of Sonoma. Similar to route 30 express, route 34 bypasses Oakmont, Glen Ellen and Sonoma Developmental Center. In the study area, route 34 serves many of the same streets that local route 32 serves, however, after it reaches the Sonoma

city limits, it travels along Highway 12, 5th Street West, Leveroni Road and Broadway to and from the Sonoma Plaza.

Route 38 provides one inter-county morning express commute trip south and one evening express commute trip north Monday through Friday between Oakmont in Sonoma County and the San Rafael Transit Center in Marin County. Other communities served along this route include Kenwood, Glen Ellen, Agua Caliente, Boyes Hot Springs, El Verano, city of Sonoma, Temelec and Schellville. Route 38 provides coordinated transfers with Golden Gate Transit's inter-county route 26 and route 80 at the San Rafael Transit Center for continued travel to and from the downtown San Francisco financial district.

It should also be noted that route 40, which is identified as a Lifeline route, provides intercity service between the cities of Petaluma and Sonoma. Route 40, however, does not provide direct service to The Springs. A transfer to or from route 40 at the Sonoma Plaza within the city of Sonoma is required for service between the study area and the city of Petaluma. Route 40 currently provides service Monday through Friday between the hours of 6:25 a.m. and 6:25 p.m. and, therefore, does not meet the suburban Lifeline objectives for hours of service during weekdays or weekends. The headways on intercity route 40 vary depending on the time of day. Service frequencies on route 40 during weekdays average 163 minutes (or every two hours and 43 minutes), which does not meet the Lifeline service objectives.

### Paratransit

Paratransit services are available within the entire the study area. Under the provisions of the Americans with Disabilities Act (ADA), public agencies offering fixed route service must offer paratransit service to eligible persons with disabilities that is "comparable" to its fixed-route system according to six service criteria: response time, passenger fares, service area, trip purpose, capacity



constraints, and hours and days of service. In the study area, the coverage area under this regulation consists of a swath three-fourths of a mile in both directions from the SCT bus routes.

Sonoma County Transit offers such paratransit service in the study area. According to Sonoma County Transit's Short Range Transit Plan FY 2008-FY 2017, increased demand for paratransit services is anticipated. There will be a need for vehicle fleet expansion and increased vehicle hours of service. SCT contracts with the Volunteer Center of Sonoma County, a non-profit organization, to provide paratransit services that comply with ADA.

(Please see map on page 33, showing bus routes and bus stops.)

### Transit Amenities

Within The Springs, there are public transit amenities located at various existing bus stops. In Agua Caliente, there are passenger waiting shelters at the bus stops located in the eastbound and westbound directions on Agua Caliente Road at Highway 12, and on Vailetti Drive at Lake Street next to the swimming center. In Boyes Hot Springs, there are passenger

waiting shelters located at the bus stops on Highway 12 across from the Boyes Hot Springs Post Office and at the Fiesta Shopping Center on Highway 12 at Siesta Way. Wooden benches are also provided at bus stops westbound on Highway 12 at Thomson Avenue and Mountain Avenue and eastbound on Highway 12 at Encinas Way. In El Verano, there is a passenger waiting shelter located at the bus stop at FAHA housing complex off of Verano Avenue. Finally, there are benches provided at bus stops on Verano Avenue at Maxwell Farms Regional Park and on Bay Street and Grove Street across from El Verano Post Office.

In addition to the passenger waiting shelters and benches described above, SCT provides and services trash receptacles at bus stops in Agua Caliente located eastbound on Agua Caliente Road at Highway 12, in Boyes Hot Springs located across from the Post Office and at Fiesta Shopping Center, and in El Verano located at Maxwell Farms Regional Park and across from the Post Office. Information panels with route schedules and maps are also included within the shelters at the bus stops located in Agua Caliente on Agua Caliente Road and on Vailetti Drive, and in Boyes Hot Springs at the Fiesta Shopping Center. Bicycle racks are provided at the bus stop located at the Fiesta Shopping Center in Boyes Hot Springs. There are ten parking spaces reserved for park-and-ride commuters at the Boyes Hot Springs public parking lot located on Highway 12 at Thomson Avenue.

All of SCT's fixed-route buses are equipped with front-loading bicycle racks, which typically accommodate three bicycles. Spaces are on a first come basis. Additional bicycles can be placed inside the bus with the consent of the bus driver, and if the bus is the last scheduled for the day.

All SCT buses are wheelchair accessible and compliant with ADA accessibility.

All of the newest SCT buses have added carrying capacity for large items (luggage, packages, etc.). This addi-

tion will more fully accommodate those who are transit dependent for shopping trips and have large items or many packages to transport.

### Spanish Language

Information about SCT services is made available in English and Spanish. Sonoma County Transit's schedule, fare and policy change notices and public hearing notices are translated into Spanish. Schedule information and public notices at bus stops are translated into Spanish in areas of Sonoma County that have been identified as having concentrations of Spanish-speaking individuals. Pictograms are installed inside all of SCT's fixed-route buses indicating basic rules for riding the bus. Several fixed-route bus operators, paratransit schedulers, and paratransit drivers are bilingual in Spanish and English; and all SCT bus operators receive minimal Spanish language training on an annual basis. Sonoma County Transit's website [www.sctransit.com](http://www.sctransit.com) is available in both Spanish and English. The website contains all of SCT's general policy information for its fixed-route bus service and paratransit service, as well as cash fare and bus pass information.

### Ridership

From November 2008 through October 2009, ridership on intercity route 30 was 133,923 passenger trips. This represented a 6.9% decrease in ridership on route 30 compared with the same time period during the previous year. Local route 32 ridership from November 2008 through October 2009 was 46,290, representing a 12.9% decrease from the previous year. Route 34 recorded 7,463 passenger trips from November 2008 through October 2009, which was a decrease of 8.3%. Route 38 had 2,897 passenger trips from November 2008 through October 2009. This was a 9.4% decrease in ridership on route 38 compared with the previous year.

Combined, routes 30, 32, 34 and 38 serving the study area had a total ridership of 190,573 from November 2008 through October 2009, which was a

decrease of 8.5% from the previous year. All of SCT's routes combined system wide, however, realized a decrease in ridership by over 8.7% during the same time period. The four routes that provide service to the study area decreased slightly less than SCT's fixed-route system as a whole during the same time period.

## LIFELINE TRANSPORTATION NETWORK

The *Lifeline Transportation Network Report* (MTC, Dec 2001) that was described in Chapter 1, was undertaken to identify a "safety net" of transportation services for those with low incomes. The report evaluated all transit routes in the San Francisco Bay Area against a set of criteria intended to identify "Lifeline Network" routes. The report identifies which public transit services, by bus route, were the most vital. Lifeline status was determined based on: 1) Service to CalWORKS clusters (California Work Opportunity and Responsibility to Kids, was established by California Assembly Bill 1542 and required each county to establish a countywide program for moving people from welfare to work); 2) Service to essential destinations; 3) Being an operator trunk route (i.e., part of their "core services), and 4) Being a regional link.

Sonoma County Transit's route 30 was selected based on categories 1 and 2. While SCT routes 20, 40, 44, 48 and 60 were also designated as Lifeline routes, none of these routes directly serve The Springs study area. Route 40 from the city of Sonoma to Petaluma was selected based on categories 2 and 3

The identification of two types of gaps was part of the report: spatial and temporal. A spatial gap exists if service to a geographic area is missing. Spatial gaps exist in areas where the target low income and/or minority are unserved by transit and do not have transit access to key destinations. In the report's analysis, no spatial gaps in service provision were identified in Sonoma County, which includes The Springs area.

Temporal gaps exist if there are time gaps in services (such as transit needs during times of the day when services are not available). None of the six Sonoma County Transit (SCT) routes identified as part of the Lifeline Transportation Network met the frequency of service objectives for all time periods during the week and on weekends.

## Service Objectives

The report established service objectives for hours of operation and frequency of service for both "Urban Core Transit Operators/Routes" and "Suburban Transit." The service objectives are broad targets that encompass the whole nine-county region, thus as such do not account for the wide variability in local circumstances, nor were associated implementation costs assigned. The study area would be considered a suburban transit route for Lifeline purposes.

### Service objectives are shown below:

#### Hours of Operation Objectives for Lifeline Routes:

##### Suburban Transit Operators/Routes:

Weekday: 6 a.m. – 10 p.m.  
Saturday: 8 a.m. – 10 p.m.  
Sunday: 8 a.m. – 10 p.m.

#### Frequency of Service Objectives for Lifeline Routes (In Minutes)

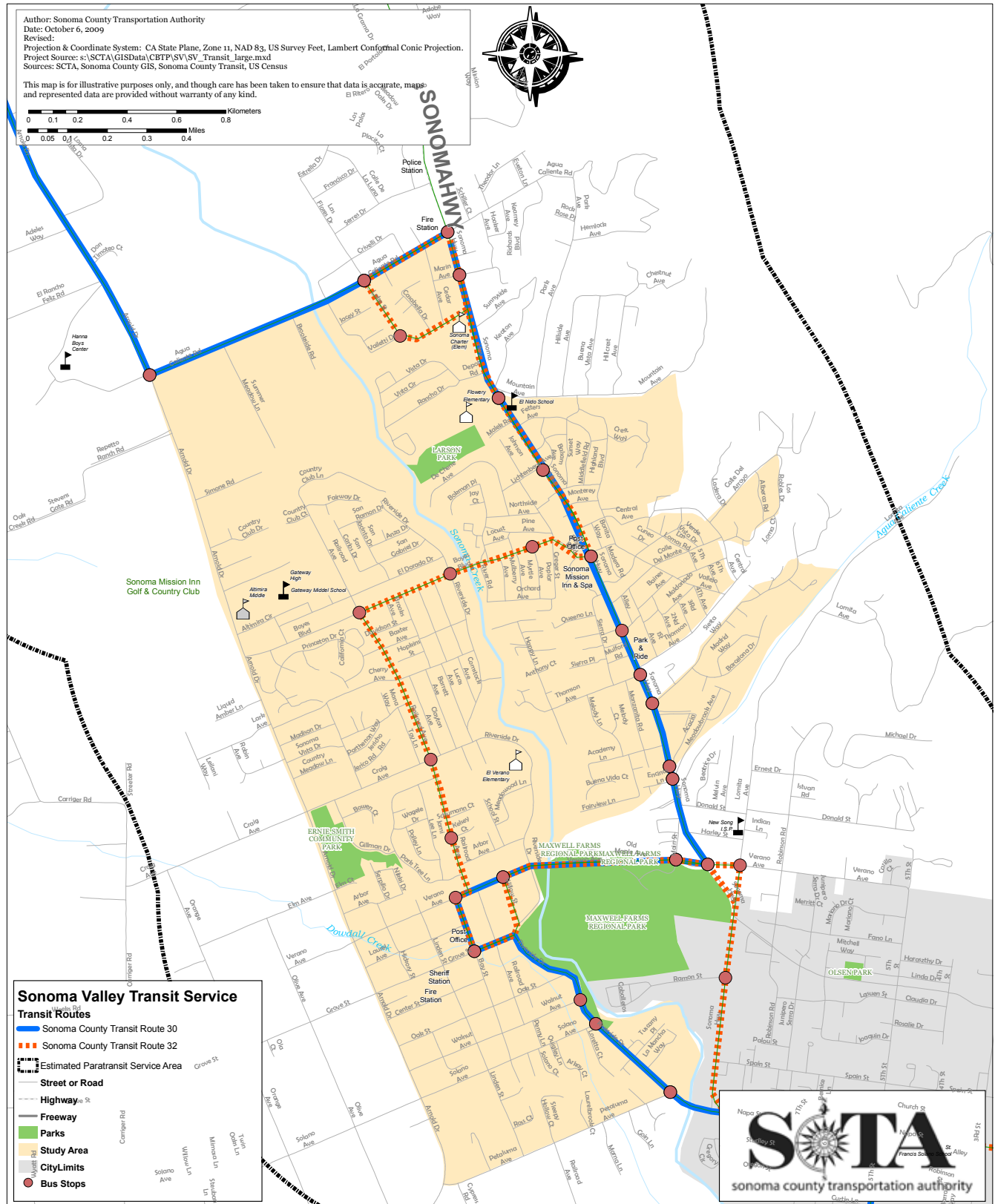
##### Suburban Transit Operators/Routes:

Weekday Commute: 30  
Weekday Midday: 30  
Weekday Night: 30  
Saturday: 30  
Sunday: 60

For the study area, potential temporal gaps in transit service were identified by comparing the span of the service day and frequency of Lifeline transit service to the suburban service objectives

Effective November 15, 2009, within the study area, intercity route 30 operates during weekdays between 6:00 a.m. and 9:15 p.m. During weekend days, route 30 provides service within the project area between 8:30 a.m. and 7:45 p.m. Local







route 32 also provides weekday service within the project area between 7:45 a.m. and 5:00 p.m. On Saturdays, route 32 operates within the project area between 9:00 a.m. and 2:00 p.m. Route 32 does not currently operate on Sundays. Route 34, however, provides additional local weekday service within the project area with one eastbound 7:20 a.m. trip and one westbound 4:50 p.m. trip. In addition, weekday commute service is provided via route 38 to and from the project area with one southbound trip at 6:05 a.m. and one northbound trip at 7:20 p.m. Lifeline Route 30 does not meet the suburban Lifeline objectives for hours of service during weekdays or weekends.

In order to compare the frequency of service for intercity route 30 within the study area to the suburban Lifeline objectives, eastbound and westbound time-points in Agua Caliente, Boyes Hot Springs and El Verano were compiled and averaged. The headways on intercity route 30 vary depending on the time of day. Service frequencies on route 30 during weekdays averages 83 minutes (or every one hour and 23 minutes) and on weekends averages 210 minutes (or every three hours and 30 minutes). Neither the average weekday frequencies nor the average weekend frequencies on intercity route 30 currently meet the Lifeline service objectives within the study area.

Although not identified as a Lifeline route, local route 32 currently provides additional weekday and Saturday service within The Springs with average weekday frequencies at every 45 minutes and average Saturday frequen-

cies at 79 minutes (or every one hour and 19 minutes). Route 30 and route 32 service combined increases average local weekday frequencies in the project area to 61 minutes (or every one hour and one minute) and on Saturdays to 145 minutes (or every two hours and 25 minutes). Although not identified as Lifeline routes, weekday intercity trips are provided within the project area via express route 34 and commute route 38. These additional morning and evening intercity trips supplement route 30 to increase average intercity weekday frequencies in the project area to approximately 77 minutes (or every one hour and 17 minutes).

TEMPORAL GAPS	WEEK DAY	SATUR DAY	SUNDAY
Objectives	6:00 a.m. - 10:00 p.m.	8:00 a.m. - 10:00 p.m.	8:00 a.m. - 10:00 p.m.
Actual Route 30	6:00 a.m. - 9:15 p.m.	8:30 a.m. - 7:45 p.m.	8:30 a.m. - 7:45 p.m.
Status	Objective Not Met	Objective Not Met	Objective Not Met

LIFELINE ROUTE 30 FREQUENCY		
	Weekday	Weekend
Frequency of Service Objective	30 minutes	30 minutes
Averaged Actual Service	83 minutes (1hr. 23")	210 minutes (3 hrs. 30")

The Sonoma County Transit Mini-Short Range Plan FY 2009-FY 2018 identifies fixed route service changes planned through fiscal year 2018. The latest plan reflects the contraction of service due to transit funding shortfalls. Transit services were substantially reduced at the end of June 2009. Minor service restorations will be considered during fiscal year 2015 and/or 2016, assuming that sufficient operating revenues are available. The city of Sonoma and County of Sonoma subsidize routes 30, 32, 34, 38 and 40. It is anticipated that new feeder bus trips to SMART passenger rail service will be introduced on routes 30 and 40 when rail service becomes available.



## REGIONAL CONNECTIVITY

### Transit

Sonoma County Transit's route 30 delivers study area customers to the Santa Rosa Transit Mall. As such, bus riders can make connections to other SCT bus routes throughout the County; Santa Rosa CityBus routes throughout Santa Rosa; and connections to out-of-county transit services. Route 30 patrons may also transfer to other SCT routes; as well as Santa Rosa CityBus routes that intersect route 30.

SCT currently provides service via route 38 to San Rafael with Golden Gate Transit (GGT) connections to San Francisco. SCT route 40 goes to Petaluma. Riders can make connections there also for south-bound GGT buses to Novato and beyond.

Golden Gate Transit runs routes from Sonoma County south to Marin County (connecting to San Francisco-bound ferries) and into San Francisco. GGT's route 80 is designated as a Lifeline Route with connections to Bay Area Rapid Transit (BART), AC Transit, MUNI, and SamTrans transit services. GGT initiated a new express service mid-June 2009. This route 101 Express operates on weekdays and reduces travel time for customers on trips destined for, or originating in, northern Marin and Sonoma counties. Travel time savings are estimated at about twenty to forty minutes depending on the time of day and the trip being made. Route 101 operates in place of route 80 on weekdays only from about 6:00 a.m. to 7:00 p.m. Route 101 serves the same stops as route 80 between Santa Rosa and Novato at the DeLong Avenue stop on Highway 101. It then runs express service to San Francisco, stopping only at San Rafael Transit Center and Spencer Avenue stop on Highway 101. Within San Francisco, Route 101 serves the same stops as route 80. In the past, GGT service has operated in Central Sonoma Valley, however service was discontinued due to low ridership in 2003.

Mendocino Transit Authority (MTA) offers service from Santa Rosa

Transit Mall to the north (e.g., to Ukiah, Willits, Fort Bragg).

Napa County Transportation and Planning Agency provides a fixed-route bus connection from Santa Rosa's downtown to the Napa Valley, however there are no routes from Sonoma Valley east to Napa.

### Air

Charles M. Schulz Sonoma County Airport in north Santa Rosa currently offers Horizon Airlines flights to Los Angeles, California; Seattle, Washington; Portland, Oregon; and Las Vegas, Nevada. Sonoma County Transit route 62 takes travelers to this airport from the Santa Rosa Transit Mall. For other destinations and international flights, travelers must utilize the San Francisco, Oakland or Sacramento airports. For three decades Sonoma Airporter offered door-to-door service from Sonoma Valley to San Francisco Airport. Due to declining revenues, the business owners announced a cessation of operations in October 2009.

Sonoma Valley has a small, privately-owned and operated airport in Schellville, which is south of the study area and city of Sonoma.

### Train

Sonoma Marin Area Rail Transit (SMART) train was approved by the voters in November 2008. Construction was to begin in 2011, with service anticipated to begin in 2014, however timeframes are under evaluation due to revenue shortfalls. The train will run within the Highway 101 corridor for seventy miles from Cloverdale to the Larkspur Ferry Terminal in Marin County, where a connection to San Francisco will be possible via existing ferry. The train stations closest by road to the study area will be those in Petaluma. Connecting bus service will be available from The Springs to stations in Petaluma and Santa Rosa.

Amtrak is the national rail service that provides a system of train routes and connecting bus services across the United States. The closest connec-

tions to Amtrak trains are provided by bus service that currently departs from Petaluma and Napa.

## **OTHER TRANSPORTATION SERVICES & ALTERNATIVES**

### **Seniors' Options**

A volunteer driver program was launched by Vintage House in February of 2009, then named the Senior Transportation and Mobility Program (STAMP), the program connects volunteer drivers with those over 60 years of age who need rides to destinations such as shops, beauty salons, barbers, banks, the library and senior center (but not medical appointments). Volunteers use their own vehicles and go through an application, screening and orientation process. The program is available to residents throughout the Sonoma Valley, including the study area. Many of the seniors served live in The Springs. In January 2010, the name of the program changed to Local Independent Mobility Options, or LIMO for short. The LIMO program provides about 60 rides a month during weekdays between approximately 9:00 a.m. and 5:00 p.m. There is an unmet need for evening and weekend rides. There is a desire to provide rides to churches, the dog park, and evening entertainment. An additional recognized unmet need is the ability to transport people who use wheelchairs. Another recognized gap is that while the program has the capacity to be bi-lingual, most Latino seniors are not aware of the program.

There are several privately-owned taxi services that operate in the study area. They offer an additional convenient option, however, many seniors and lower-income residents find taxi fares expensive.

### **Car- and Van-Pooling and Car Sharing**

According to the 2000 Census, an average of fifteen percent of study area residents carpool to get to work. Carpooling, of course, can be used for various trip types in addition to work trips. This travel mode is often informal in nature and is arranged

through networking among families, friends, co-workers and church members. Rides can also be arranged through MTC's 511.org rideshare program or community bulletin boards. Such travel arrangements yield savings in car operation and ownership costs, as well as mitigation of environmental impacts. One outreach recommendation was to encourage Santa Rosa Junior College students to utilize carpooling arrangements.

### **Motorcycle/Scooters**

Per the 2000 Census, one percent of employed study area residents used motorcycles to get to work. The percentage of use for all trip types is not known. It is possible that motorcycle or motor scooter use could be viable alternatives for more people, and one that offers potential environmental and cost-savings benefits over solo use of automobiles.

## CHAPTER 3

# OUTREACH STRATEGY

## OVERVIEW OF STRATEGY

### Methodology

Data gathering methods included field observations, obtaining input from a body of stakeholders, direct public outreach in the community, and leveraging other planning efforts. Direct public outreach consisted of administration of a survey questionnaire at key gathering points in the study area, individual interviews, and an evening meeting conducted to invite additional public participation and input. Outreach components were:

- Field Observations
- Stakeholders Committee
- Direct Public Outreach in the Community
  - Surveys
  - Individual Interviews
  - Public Meeting
- Leveraging Other Planning Efforts

Input derived from this aggregation of data gathering was used to identify gaps and issues in transportation and corresponding potential transportation improvements for The Springs. Solutions—some projects and some

strategies—are included in Chapter Five of this plan, the “action plan” component.

This planning effort has involved the community through outreach to residents, employers, community-based organizations, faith-based organizations, transportation and service providers, governmental agencies, and the business community. This outreach has been consistent with guidelines of MTC’s Community Based Transportation Planning Program.

### FIELD OBSERVATIONS

Field observations were undertaken to gain first-hand exposure to existing conditions; to determine boundaries of the study area; locate low-income housing areas, shopping, social services, and school centers; and to scope out survey sites. Additionally, Sonoma County Transit bus trips were made to observe service areas and understand schedules and routes, as well as to administer surveys enroute.

### STAKEHOLDERS COMMITTEE

The first step in conducting outreach was to convene a stakeholders committee to advise the planning effort. Stakeholders

advised the outreach strategy, provided input into issue identification, served as resources for the plan's content, and facilitated selection of solutions. The approach taken for stakeholder selection was based on engaging people who had a stake in study outcomes. These were identified as people who are:

- Residents of the study area
- Providers of services within the study area
- Employers within the study area
- Involved in planning efforts within the study area

Three Springs Stakeholders Committee meetings were held at the La Luz Center in Boyes Hot Springs on:

- October 8, 2009
- January 14, 2010
- March 31, 2010

Eighteen people participated at one or more of the stakeholder committee meetings. The following are members of The Springs CBTP Stakeholder Committee:

	NAME	ORGANIZATION
1	Michael Acker, Artist, Resident	Community representative
2	Zuli Baron, Community Organizer	St Joseph's Health System
3	Mario Castillo, Director	La Luz Center's Vineyard Workers Services & Community Relations
4	Ellen Conlan, Supervisor's Appointee	Redevelopment Advisory Committee
5	Stephen Dale, Executive Director	La Luz Center
6/7	Father Donahue, Priest, Bill Thomas (alternate)	St Leo's Catholic Church
8	Edwin Ferran, ARRA Special Projects Manager, Volunteer	Community Action Partnership of Sonoma County, Valley of the Moon Teen Center

	NAME	ORGANIZATION
9	Kevin Howze, Engineering Division Manager	Sonoma County Transportation & Public Works
10	Rich Lee, Business Property Owner	Redevelopment Advisory Committee, Sonoma Valley Chamber
11	Al Lerma, Redevelopment Associate	Sonoma County Community Development Commission. The Springs Redevelopment Area
12	Kara Reyes, Family Advocacy Director, Supervisor's Appointee	La Luz Center, Redevelopment Advisory Committee
13	Chip Roberson, Citizen Representative	Countywide Bicycle & Pedestrian Advisory Committee
14	Cynthia Scarborough, Executive Director	Vintage House Senior Center
15	Steven Schmitz, Senior Transit Planner	Sonoma County Transit
16	Jerome C. Smith, MD	Sonoma Valley Community Health
17	Ken Tam, Park Planner	Sonoma County Regional Parks
18	Jennifer Yeaman, Lifeline & Equity Planner	Metropolitan Transportation Commission: Community Based Transportation Planning Program

Due mainly to work constraints, a number of those invited to participate as stakeholders were unable to do so. These representation gaps were filled by inviting those unable to participate in stakeholder meetings to become interviewees.

### First Meeting

At the first meeting, after the project was introduced and its funding and purpose were detailed, stakeholders were charged with providing input regarding stakeholder selection and who they thought would be good candidates for interviews—specifically how gaps in representation could be filled. The names of individuals, entities and organizations were gathered for future contact. Gaining the commitment

of lower-income residents to participate was difficult in this process and they were reached largely through the other components of the outreach strategy.

Stakeholders were also asked to comment on the overall outreach strategy, including the draft survey instrument. Members recommended alterations to the survey and methods to administer the survey. This included specific locations and dissemination vehicles to reach people, including those in the Latino community, workers, and seniors. The later part of the meeting was devoted to beginning an identification of problems and potential solutions—drawing on stakeholders' knowledge of the community.

### Second Meeting

At the second meeting, outreach methodologies were reviewed and outreach findings were presented and discussed. The group was asked to validate, clarify, and augment the findings based on their knowledge of, and experience in, the study area. At this meeting, "homework" was assigned to refine and elaborate on the parameters of potential solutions. Stakeholders taking on "homework" represented the various entities that could implement solutions. Potential solutions were derived directly from outreach findings. A template was provided to each participant to place each solution in a uniform format to state what problem was being addressed; what solution was being proposed; what resources (funding and participating entities) would be required for implementation; what implementation would consist of including timeframe; what barriers to success exist; and who would benefit by solution delivery. The body was also asked to review a proposed methodology to be utilized in evaluating projects and strategies proposed as solutions.

### Third Meeting

Evaluation of solutions and priority setting were goals of the third and last Stakeholders Committee meeting. At this stakeholders meeting proposed solutions

(projects and strategies) were presented for consideration. The committee applied an evaluation criteria and selection methodology to solutions after they were described and discussed. Prioritization of the potential solutions was completed.

## DIRECT PUBLIC OUTREACH IN THE STUDY AREA

During the months of October, November and December 2009, the Sonoma County Transportation Authority (SCTA) with the consulting support of Matt Stevens (The Results Group) conducted public outreach in an effort to gather data on how residents and community-based service providers experienced and thought about transportation in The Springs area of Central Sonoma Valley.

### Surveys

One hundred and fifty-nine people completed the survey questionnaire. Two versions of the survey instrument were distributed: one in English and one in Spanish. During most tabling sessions the survey taking was facilitated by Spanish language speakers who were on hand to assist survey takers as needed. This was important as some within the low-income Latino population have difficulty reading and/or writing in Spanish. In fact the first language of some immigrants from Mexico or Central America may be a regional dialect.

Significantly, 72.2% of those surveyed were determined to have incomes at or below the study area definition of poverty, specifically at or below 200% of the federal poverty level based on stated incomes and household size.

Surveys were administered at six locations in The Springs study area: La Luz Center, El Verano Post Office, The Springs Apartment Complex, Fiesta Shopping Center, and at The Springs Town Hall Meeting held at The Grange. Surveys were also administered on the Sonoma County Transit Route 30 bus.

The Executive Director Stephen Dale and staff of the La Luz Center, and



Zuli Baron of the St. Joseph Health System, provided direct support in the administration of the surveys. The SCTA is sincerely appreciative of this assistance in obtaining quality input.

Survey takers included the following:

- Sonoma County Transit bus patrons of route 30 [Lifeline Route] in transit to, from, and in the study area
- People in the vicinity of the El Verano Post Office
- People in the vicinity of the Sonoma Valley Bank in the Fiesta Shopping Center (Saturday a.m.)
- Residents of The Springs Village low-income housing complex
- People using the services of La Luz during a scheduled food distribution
- Residents attending The Springs Town Hall meeting

### Individual Interviews

Individual interviews with people intimately involved in the community were invaluable in gaining an understanding of the issues. All of the interviewees serve the community in some capacity. Interviewees may interface with the study area low-income people as residents of the area, constituents, workers, social services clients, church members, school children, homeless persons, and seniors. Interviews were conducted mainly in person and sometimes by telephone. The following lists the entities and individuals who were included in the interview component of the CBTP:

INTERVIEW PARTICIPANTS	
Entity	Interviewee
St Joseph's Health System, Community Organizer	Zuli Baron
Sonoma Valley Unified School District, Transportation Manager	John Bartolome
Fairmont Sonoma Mission Spa and Inn, Director of Operations	Michelle Bertram

INTERVIEW PARTICIPANTS	
Sonoma County Board of Supervisors, SCTA Board of Directors	Valerie Brown, Board Member, SCTA Director & Vice Chair
Sonoma Valley Unified School District, Transportation Dispatch	Vernice Bruno
Springs Task Force Coordinating Committee (STFCC) Representative, Redevelopment Advisory Committee: Representative of a Community Organization	Steven Cox
The La Luz Center, Executive Director	Stephen Dale
The Springs Town Hall Organizer, Study Area Business Owner: The Epicurean Connection	Sheana Davis
Saint Leo's Catholic Church, Priest	Father Donahue
SCTA Board of Directors, Sonoma Charter School Staff	Laurie Gallian, SCTA Director
Senior Transportation and Mobility Program (STAMP), Former Sonoma City Manager and Business Owner	Pamela Gibson
Valley of the Moon Teen Center, Executive Director	Rebecca Hermosillo
Sonoma Valley Chamber of Commerce, Executive Director, Also Volunteers with FISH	Jennifer Yankovich

### Public Meeting to Disseminate Findings and Receive Feedback

The final outreach component consisted of a public meeting on April 19, 2010, at The Sonoma Valley Grange in Boyes Hot Springs. The evening meeting was advertised by means of e-mailings, flier postings in the study area, and a news article in the local press, The Sonoma Index-Tribune. The meeting consisted of a presentation of the findings and proposed "solutions," discussion, and a request for feedback. Questions were



answered about the plan and participants' comments were recorded.

### **LEVERAGING OTHER PLANNING EFFORTS**

A number of those who contributed to this plan have been involved in The Springs Redevelopment Advisory Committee (the RAC). The Springs Redevelopment Project Area Strategic Plan was approved in March 2007, following a planning process beginning in late 2005 involving the community in setting a vision and priorities for improvement. Outreach conducted as part of this planning process offered additional public guidance to this CBTP effort. It also validated the findings of this CBTP planning effort, in that pedestrian safety issues emerged as of primary concern. Priority actions included in this plan pertaining to transportation are:

- Construct Highway 12 improvements (the plan's top priority)
- Conduct a parking analysis
- Install crosswalks with flashing lights
- Require pedestrian amenities along Highway 12

California Redevelopment Law defines activities for the use of redevelopment funding. Related to transportation, allowable improvements include roads, sidewalks, bicycle lanes, lighting, and landscaping. The recently completed Highway 12 improvements were funded by this source; as will be future improvements. Redevelopment revenues have been "banked" for many years to make this possible.



## CHAPTER 4

# IDENTIFICATION OF PROBLEMS & POTENTIAL SOLUTIONS

## OVERVIEW OF PROCESS

Identification of transportation problems and potential solutions for the study area involved outreach to the public as was described in Chapter Three. After compiling and presenting the “raw input” to the Stakeholders Committee, members were asked to fill in any additional gaps and offer any new potential solutions. Members representing potential implementing agencies were then asked to describe and define potential solutions by crafting projects and strategies. These solutions are presented and prioritized in Chapter Five, the “action plan” component of the CBTP. Compiled and summarized in this chapter are the findings of the public outreach.

## OVERVIEW OF PUBLIC INPUT

Input provided by survey respondents, interviewees, and stakeholders was quite consistent in focus; as well as in character with the conclusions of other planning efforts pertaining to the area. There was much enthusiasm for the recently completed Highway 12 renovations. The highest priority for future work is completion of Highway 12 improvements along the entire length of the facility; and secondly further improve-

ments to increase the safety of people who walk in the study area. This took the form of requests for more sidewalks and night time lighting of streets and parks. Making bicycle travel more feasible and safe was also frequently mentioned. Other common themes stated as needs were: greater frequency of bus service; adequate parking for local businesses; relief of traffic congestion on Highway 12, and a higher level of pavement maintenance. A repeated suggestion was provision of a shuttle/jitney on a local route that would circulate through the study area on a frequent schedule.

## KEY OUTREACH FINDINGS

### Summary

There is variation in the length of time in the country/county, nationality, legal status, educational level, and economic status of community members. For purposes of this plan, focus was placed on lower-income residents, and it is this group that is being referenced.

Springs study area survey takers were predominately Latino, whose incomes fall typically below 200% of the Federal poverty level (72.2% of survey respondents).

- A high percentage of survey takers indicated that they don't own cars (47.9%).
- A significant proportion of survey respondents walk and ride bicycles, car-share and car-pool as primary modes of transportation (37.7% of survey respondents stated they don't drive and 34.6% don't have cars).
- Pedestrian facilities (side-walks) within The Springs need significant improvements.
- Bicycle facilities within The Springs need significant improvements.
- Highway 12 improvements (Phase I) are greatly appreciated and there is a desire for Phase II improvements to be made as soon as possible.
- Three key roadways in The Springs are in great need of improvement: Arnold Drive, Agua Caliente Road and Boyes Boulevard.
- Bus service improvements are desired: more frequent service to key destinations (Sonoma, Napa, Santa Rosa and Petaluma).
- More weekend and evening service is also needed by the Latino community to meet evening and night shift and weekend work transportation needs
- A local circulator was frequently mentioned as a needed transit improvement.
- Pedestrian lighting is needed to improve safety in key areas: Larson Regional Park, Highway 12 (north and south sections), Agua Caliente Road and Boyes Boulevard.
- Congestion has increased on Highway 12. Implement strategies to provide congestion relief.
- Seniors who live in The Springs need expanded transit service to enable enrichment, and improved transit service to health care services.

## TOP SURVEY RESULTS

The survey asked people if they experienced difficulty in getting to their destinations, what kind of problems they experienced, and what improvements they would like.

Percentage of people who experience difficulty getting to where they want to go:

Never: 7.9%

Sometimes: 51.3%

Often: 18.4%

Always: 22.4%

When asked to specify which destinations they had difficulty reaching, 57.9% indicated shopping as difficult. Health services were indicated next by 45.3%; school by 41.5%; jobs by 31.4%; and religious activities by 28.3%. Government, senior and childcare services were all indicated by less than 8%.

### Kinds of problems:

- Walking feels unsafe due to inadequate sidewalks
- Bicycling feels unsafe due to inadequate bicycle facilities
- Don't have a driver's license
- Don't have access to a car
- Need bus service to Napa and Vallejo
- Bus schedules need to be available at bus stops
- No weekend and evening bus service

### Most important improvements:

- Add or improve sidewalks
- More frequent bus service
- Weekend and evening bus service
- Add bicycle pathways
- Add bus service to key destinations (Napa and Vallejo)

## SPECIFIC PROBLEMS AND SOLUTIONS

The following describes specific problems identified in the study area and potential solutions to those problems. Community-identified problems and suggested solutions herein are opinions, which may or may not be feasible or preferable after all pros and cons are considered. Emphasis is on pedestrian and bicycling issues as there is a large Latino population living in the area that walks and/or rides bicycles as their predominant modes of transportation. For example, the Fairmont Sonoma Mission Inn and Spa has approximately 200 employees who ride bicycles or walk to work. Other categories relate to transit and road services and operations.

PEDESTRIANS	
Problem	Pedestrian safety emerged as a priority concern. Inadequate sidewalks were identified in several parts of the study area. Survey respondents stated that they felt that walking was unsafe due to the poor condition of sidewalks (49.1%) and the lack of sidewalks (52.2%). Respondents frequently mentioned seeing mothers walking with small children along busy roadways where there are no sidewalks, or inadequate roadway shoulders.
Solutions	Implement a Safe Routes to School program for The Springs area schools (Flowery, Sonoma Charter, Altamira and El Verano). Install sidewalks on Agua Caliente Road (between Highway 12 and Vailetti Drive; Vailetti Drive to Arnold Drive). Install sidewalks on Boyes Boulevard from Highway 12 to Arnold Drive. Install sidewalks on Riverside Drive. Complete sidewalks on Verano Avenue from Highway 12 to Arnold Drive. Install sidewalks on Bay and Grove streets in vicinity of El Verano Post Office. Install sidewalks or a wider shoulder in vicinity of Altamira School to Agua Caliente Road. Sidewalks on cross-streets (to Highway 12) and neighborhoods as they do not have any sidewalks. Sidewalk on the route most teens take when walking to the teen center.

Build sidewalks wide enough to accommodate mothers with several children.
<b>Problem</b>
Pedestrian crossing safety is an issue along Highway 12 and at other main thoroughfares in the study area.
<b>Solutions:</b>
Address crossing of Highway 12 at Donald Street by Mary's Pizza.
Address crossing of Highway 12 by Sonoma Charter School.
Address crossing of Highway 12 Thomson Street.
Address crossing of Highway 12 at Food Center (Arroyo Road and Calle Del Monte)
Address crossing of Highway 12 Central Avenue.
Address crossing of Verano Avenue at Riverside Drive.
Crosswalks are needed every six to eight blocks across Highway 12.
<b>Problem</b>
Inadequate pedestrian lighting is an issue.
<b>Solutions</b>
Install lighting at Larson Regional Park.
Install lighting from the Fruit Basket to Verano Avenue on Highway 12.
Repair/install lighting around Valley of the Moon Teen Center.
Install lighting along Agua Caliente Road (between Highway 12 and Vailetti Drive; Vailetti Drive to Arnold Drive).
Install lighting along Boyes Boulevard from Highway 12 to Arnold Drive.
<b>BICYCLE FACILITIES</b>
<b>Problem</b>
Bicycle safety is an issue in several parts of the study area. 58.5% of survey respondents stated that they felt bicycling was unsafe due to the lack of bicycle facilities. The lack of lighting was cited as a safety issue for night time bicycling.
<b>Solutions</b>
Widen road and add class II bicycle lanes on Arnold Drive (Altamira School to Glen Ellen) (specifically, Country Club Drive to Agua Caliente Road in the study area).
Class II bicycle lanes on Agua Caliente Road (between Highway 12 and Arnold Drive).
Class II bicycle lanes on Boyes Boulevard (between Highway 12 and Arnold Drive).

Class II bicycle lane gap closure on Verano Avenue at bridge crossing of Sonoma Creek.
Expand bike/pedestrian route/trail system from the current central Sonoma trail system (functioning as the trunk) with feeder pathways coming from schools and housing.
Initiate bicycle safety education campaign.
Implement a Safe Routes to School program for The Springs area schools (Flowery, Sonoma Charter, Altimira and El Verano).
Install more lighting along bicycle paths, lanes, and routes.
<b>Problem</b>
Secure bicycle parking is not easily found. Many residents ride bicycles to work in the area and there are few bicycle parking facilities in the area. 43.4% of survey respondents stated that there was no bicycle parking at their destinations. (It should be noted that the Fairmont Sonoma Mission Inn and Spa provides secure bicycle parking for their employees.)
<b>Solutions</b>
Survey feasibility of installing more bicycle parking on public and private properties.
Provide incentives for businesses to install bicycle parking.
Increase bicycle parking at transit stops.
<b>BUS TRANSIT</b>
<b>Problem</b>
Bus transit service does not go where riders want to go. The following destinations were indicated as places study area residents want to go: Napa Vallejo San Francisco Petaluma Sonoma Santa Rosa
<b>Solutions</b>
Increase service frequency and/or develop additional bus routes to these destinations.
Restore service to San Francisco and airport.
Create new routes to Napa and Vallejo.
Utilize carpooling to Santa Rosa Junior College as an alternative.
Utilize the volunteer driver program, LIMO, as an alternative.
<b>Problem</b>

Riding the bus takes too long and is not convenient. Specific complaints:
Route 30 takes too long and has insufficiently frequent headways.
Buses are unreliable and are often late. Riders arrive late to work.
<b>Solutions</b>
Increase frequency of headways of Route 30.
Increase frequency and efficiency of Route 32 in The Springs.
Improve reliability of bus service.
Provide a frequent local circulator in the form of a shuttle or jitney.
<b>Problem</b>
Transit service limited hours resulting in the following gaps:
Hampered ability of bus riders to get to and from work on the weekends.
Hampered ability of bus riders to get to and from work during the evening.
Preventing seniors from taking advantage of evening entertainment needed for enrichment.
Difficulty accessing church services on Sundays.
<b>Solutions</b>
Expand transit service into evening hours.
Expand transit service on weekends.
<b>Problem</b>
Bus riders experience the following bus driver problems:
Some bus drivers are not patient with Spanish speaking riders.
Some drivers do not understand Spanish.
Some bus drivers are rude; do not greet riders.
<b>Solutions</b>
Increase efforts to deliver quality customer service, including Spanish language ability.
<b>Problem</b>
Bus riders don't know where to get schedule and route information. Bus schedules are not easily found at stops.
<b>Solutions</b>
Improve the way bus schedules are distributed and displayed at bus stops.
Conduct outreach to potential Latino bus patrons to facilitate transit use
<b>Problem</b>
Many bus stops have no shelters and/or benches.
<b>Solution</b>



Install more benches and shelters throughout study area.

### LOCAL ROADS/HIGHWAY 12 ROADS AND ROADWAY OPERATIONS

#### Problem

Parking along Highway 12 has become inadequate since Phase I improvements have been implemented. Phase II implementation needs to consider how adequate parking is to be maintained.

#### Solutions

Conduct a parking study to determine where adequate parking may be developed.

#### Problem

Road conditions (e.g., the quality of pavement) are deteriorated throughout the study area.

#### Solutions

Secure funding to improve pavement maintenance on County roads.

#### Problem

Traffic congestion on Highway 12, especially during commute peaks.

#### Solution

Intelligent signal coordination of Highway 12.

Complete Phase II roadway improvements on Highway 12.

Improve Arnold Drive as a parallel alternative.

#### Problem

Need for traffic management and safety at intersection of Arnold Drive and Agua Caliente Road.

#### Solution

Signalize the intersection.

### Alien Residents Unable to Obtain California Driver's License

While resolution of this concern is beyond the scope of this planning effort, an additional issue bears mentioning because it has an impact on the mobility options of some low income study area residents, in addition to having potential public safety impacts.

The California Vehicle Code states that to obtain a drivers license, an applicant must submit satisfactory proof that their presence in the United States is authorized under federal law. Without a valid driver's license it is difficult, if not impossible, for undocumented alien residents to open bank accounts or

access credit. Many purchase inexpensive, substandard, non-compliant vehicles from non-traditional sources. To register such vehicles, proofs of insurance and passing smog tests are required. These seemingly simple requirements are unobtainable to the undocumented alien resident. Driving cars without vehicle registrations, insurance, and/or driver's licenses creates circumstances that can lead to various costs and problems, arrests, and vehicle impoundments.

### PUBLIC MEETING RESULTS

As a component of the CBTP public outreach, a meeting was held to gain additional public comments and input on priorities. Eight people attended the evening meeting on April 19, 2010 at The Sonoma Valley Grange. The opportunity had been advertised by means of fliers in the windows of local businesses, e-mail invitations, and a news article in The Sonoma Index-Tribune. The meeting was comprised of a project overview, presentation of the public outreach findings, discussion of the solutions and their prioritization by the stakeholders (as described in Chapter 5), an invitation to comment on and add to the identification of problems and solutions, and a question and answer close. The group verified that the priorities determined by the stakeholders seemed correct and useful. The following summarizes the group's additional comments.

#### Pedestrian/Bicycle Comments:

- Sidewalks made along Highway 12 are appreciated
- Complete Phase II improvements as soon as possible
- Sidewalks are definitely needed on many roads crossing Highway 12
- Sidewalks are a critical community enhancement
- Directional indicators (arrows) in Class II bike lanes would be helpful

**Bus Service Comments:**

- A more frequent local circulator transit service would be great
- An initiative to increase the public's awareness of transit is needed
- Route 32 buses could be repainted/rebranded for local service.
- La Luz is willing to participate in a transit education/marketing outreach effort
- Remember that there is a broad population that would use a circulator bus
- The city of Sonoma will be working with Sonoma County Transit to promote transit use in the Sonoma Valley

## CHAPTER 5

# ACTION PLAN FOR IMPLEMENTATION

## INTRODUCTION

This chapter transforms the public input into solutions that could be implemented to provide a benefit to the low-income residents of The Springs study area. It also provides information to the public regarding the feasibility of potential solutions and adds context to some of the identified problems. Herein is a prioritized list of projects and strategies that offers an action plan of solutions for potential implementation

While the current economic downturn makes implementing projects and improvements difficult, there is value in having plans in place to offer guidance on what the public priorities are, and to put forth ideas about a variety of potential approaches that may assist in addressing problems. It can be assumed that implementation of some of the proposed solutions, such as major transit enhancements, would be dependent not only on a resumption of a more normal fiscal forecast, but an augmentation of transit funding.

## SOLUTIONS FORMULATION

Chapter Three described the outreach strategy that was used to garner public input into this planning effort; Chapter

Four presented outreach findings to reveal public opinions about problems and solutions. Aggregated findings were presented to the stakeholder body and discussed. A framework was also refined to facilitate evaluation and prioritization of proposed solutions. Stakeholders who represented agencies that could potentially be implementers of solutions took on the task of more fully describing and defining what projects or strategies could be considered for implementation. A template was supplied to facilitate an evaluation of solutions.

Not every outreach result was translated into a project or strategy. For example, there was the concept of having a jitney-style circulator; however, no potential implementing entity could be identified. Furthermore, increasing the frequency of the local bus service is more feasible as a solution (see Solution A below). Additionally, there was outreach indicating that improved pavement maintenance, and many more sidewalks are desired (e.g., the length of Riverside Drive and in the area of the El Verano Post Office). There are many variables in assigning costs and achieving implementation of such broadly stated ideas for improvement. It is not implied that these concepts are not valid, only that more would need

to be known regarding parameters of improvements in order to make estimates and consider the pros and cons of these projects. Several identified needs were not included because implementation is already progressing. These are described after the solution sets are presented.

## **SOLUTIONS EVALUATION & PRIORITIZATION**

Potential solutions were evaluated through seven criteria lenses, with assigned scores of high, medium or low for each lens. The evaluation set was utilized not as a mathematical exercise, but rather as a decision support tool to evaluate solutions and determine priority. The seven lenses are described below.

### **CRITERIA LENSES**

#### **Community Support**

**Definition:** Priority based on CBTP community outreach.

**High:** Among most frequently identified needs.

**Medium:** In mid-range of identified needs.

**Low:** Among least frequently identified needs.

#### **Implementation Feasibility**

**Definition:** Funding availability and funding sustainability

**High:** Probable funding source identified, funding may be readily available and project can be sustained

**Medium:** Possible funding source identified, funding may be available and project can be sustained

**Low:** Probable funding source not identified; funding may be difficult and project possibly can not be sustained

#### **Cost/ Benefit**

**Definition:** Number of beneficiaries, number of gaps closed, measurable results and contribution to economic vitality to the commu-

#### **nity and well-being of low-income people compared to the cost**

**High:** Significant increase in number of low-income people served and identified gaps closed

**Medium:** Moderate increase in number of low-income people served and identified gaps closed

**Low:** Minimal increase in number of low-income people served and identified gaps closed

#### **Public Health Benefits**

**Definition:** Supports beneficial health behaviors

**High:** High positive health benefits

**Medium:** Neutral health benefits

**Low:** Low or negative health benefits

#### **Environmental Benefits**

**Definition:** Net reduction in pollution, resource use, greenhouse gas emissions

**High:** Positive environmental benefits

**Medium:** Neutral

**Low:** Low or negative environmental impacts

#### **Mobility/Accessibility/Reliability**

**Definition:** Transportation utility in terms of reaching jobs, education, childcare, needed services and access to recreation

**High:** Significant increase in providing mobility; greater access to desired locations/services; enhanced transportation reliability

**Medium:** Moderate increase in providing mobility; greater access to desired locations/services; enhanced transportation reliability

**Low:** Low increase in providing mobility; greater access to desired locations/services; enhanced transportation reliability

## Safety/ Security

**Definition: Transportation user safety and security (bicyclists, pedestrians, motorists, seniors, school children, transit users, etc.)**

**High:** Significant increase in providing safety and/or security

**Medium:** Moderate increase in providing safety and/or security

**Low:** Neutral in providing safety and/or security

## SOLUTIONS SELECTION & RANKING

Twenty-five solutions (projects or strategies) were identified that respond to the public outreach findings. Solutions were proposed in three broad categories. At the March Stakeholders Committee meeting solutions were described, discussed, and a prioritization strategy was decided upon and utilized. Two of the solutions (comprising a “traffic operations” category) were already being implemented, thus twenty-three solutions remained for ranking. All of these fall into one of two categories: 1) Transit/Paratransit, or 2) Pedestrian/Bicyclist.

The following table shows the “action plan” solutions. The number in column three indicates the final ranking. All of the solutions had ties, for example solutions A and B are equally ranked as #1 in priority; C and D as #2, etc.

SOLUTIONS SHOWING RANKING		
Highest Priority		
A	Increase frequency of route 32 buses to/from The Springs and Sonoma	1
B	Safe Routes to Schools program	1
C	Complete the Central Sonoma Valley Bikeway (Class I, multi-use path)	2
D	Bicycle Education Campaign & Street Skills classes	2
E	Maintain existing levels of transit service	3
F	Enhance pedestrian crossings on Highway 12 at various locations	3
G	Install more shelters, benches & bike racks at bus stops	3

## SOLUTIONS SHOWING RANKING

H	Expand outreach & customer service efforts to potential & existing Latino bus patrons	3
Medium Priority		
I	Increase frequency of route 40 buses to/from The Springs & Petaluma, including Saturday service	4
J	Increase frequency of route 30 buses to/from The Springs & Santa Rosa & Sonoma	4
K	Complete Verano Avenue sidewalks from Highway 12 to Sonoma Creek	4
L	Arnold Drive bicycle lanes from Agua Caliente Road to Country Club Drive	4
M	Implement a new weekday bus route between the cities of Sonoma & Napa	5
N	Agua Caliente Road bicycle lanes from Highway 12 to Arnold Drive	5
O	Boyes Boulevard sidewalks from Highway 12 to Arnold Drive	5
P	Boyes Boulevard bicycle lanes from Highway 12 to Arnold Drive	5
Lower Priority		
Q	Later afternoon and/or evening bus service & expanded ADA paratransit service	6
R	Pedestrian lighting on Highway 12 from Donald Street to Verano Avenue	6
S	Agua Caliente Road sidewalks from Highway 12 to Vailetti Drive	6
T	Provide incentives for businesses to provide safe & convenient bicycle parking	6
U	Reinstitute Golden Gate Transit route 90 bus service from Sonoma Valley to San Rafael & San Francisco	7
V	Add pedestrian crossings on Verano Avenue at Riverside Drive	7
W	Pedestrian lighting Agua Caliente Road & Boyes Boulevard	7

## SOLUTIONS BY CATEGORY

### Transit/Paratransit

Increase frequency of route 32 buses to/from The Springs and Sonoma
Maintain existing levels of transit service
Expand outreach & customer service efforts to potential & existing Latino bus patrons
Increase frequency of route 40 buses to/from The Springs & Petaluma, including Saturday service
Increase frequency of route 30 buses to/from The Springs & Santa Rosa & Sonoma

Implement a new weekday bus route between the cities of Sonoma & Napa
Later afternoon and/or evening bus service & expanded ADA paratransit service
Reinstitute Golden Gate Transit route 90 bus service from Sonoma Valley to San Rafael & San Francisco
<b>Pedestrian/ Bicyclist</b>
Safe Routes to Schools program in study area
Complete the Central Sonoma Valley Bikeway (Class I, multi-use path)
Bicycle Education Campaign & Street Skills classes
Enhance pedestrian crossings on Highway 12 at various locations
Install more shelters, benches & bike racks at bus stops
Complete Verano Avenue sidewalks from Highway 12 to Sonoma Creek
Arnold Drive bicycle lanes from Agua Caliente Road to Country Club Drive
Agua Caliente Road bicycle lanes from Highway 12 to Arnold Drive
Boyes Boulevard sidewalks from Highway 12 to Arnold Drive
Boyes Boulevard bicycle lanes from Highway 12 to Arnold Drive
Pedestrian lighting on Highway 12 from Donald Street to Verano Avenue
Agua Caliente Road sidewalks from Highway 12 to Vailetti Drive
Provide incentives for businesses to provide safe & convenient bicycle parking
Add pedestrian crossings on Verano Avenue at Riverside Drive
Pedestrian lighting Agua Caliente Road & Boyes Boulevard

## SOLUTIONS SETS

These proposed solutions are described in detail below:

### SOLUTION A: INCREASE FREQUENCY OF ROUTE 32 BUSES TO/FROM THE SPRINGS AND SONOMA

#### PROBLEM DEFINITION

Frequency of service for route 32 during weekdays and Saturdays is somewhat limited. Sonoma County Transit's service frequency on route 32 during

weekdays currently averages every 45 minutes, and on Saturdays every one hour and fifteen minutes.

#### DESCRIPTION

Increasing the frequency of service (decreasing headways) on Sonoma County Transit's route 32 between The Springs and Sonoma to every thirty minutes on weekdays and to every fifty minutes on Saturdays, for example, would require a substantial amount of additional funding. As an alternative to additional funding, bus routes in other parts of Sonoma County Transit's service area could be reduced substantially or completely eliminated to accommodate increased frequencies on route 32. However, prior to any such changes, ridership counts and passenger surveys on routes throughout Sonoma County Transit's fixed-route bus system would need to be conducted and analyzed to determine how they might impact passengers.

#### IDENTIFY NEEDED RESOURCES:

- Estimated cost: \$200,000 annual cost (2010 dollars...assumes 33% increase in existing route 32 weekday and Saturday service hours).
- Potential funding sources: Transportation Development Act, Measure M, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, city of Sonoma.

#### DISCUSS IMPLEMENTATION

#### TIMEFRAME

Service could be gradually implemented over several years if an adequate and on-going funding source were to be secured.

#### BARRIERS TO SUCCESS

Lack of funding.

#### BENEFICIARIES

Route 32 passengers traveling in the study area and between The Springs and Sonoma.

### SOLUTION B: SAFE ROUTES TO SCHOOLS IN THE STUDY AREA

#### PROBLEM DEFINITION

Children in The Springs need to be able to move about safely in their neighborhoods and to schools. Dangerous and difficult access to schools by bicycling and walking forces more residents to use automobile transportation. It is important to create greater viability of alternative modes to



automotive travel as a strategy to reduce greenhouse gas emissions and air pollution, as well as contributing to healthier lifestyles for children.

### DESCRIPTION

Safe Routes to Schools is a program designed to decrease traffic and pollution, and increase the health and safety of children and their community. The program promotes walking and bicycling to school through education and incentives. The program also addresses safety concerns of parents by encouraging greater enforcement of traffic laws, educating the public, and exploring ways to create safer streets.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: Approx. \$25,500 per school per year (based on current Sebastopol program and Sonoma County Department of Public Health grant).
- Potential funding sources: Federal, State SRTS grants, Measure M, Office of Traffic Safety, foundation grants, potential vehicle licensing fee, MTC Regional SR2S.
- What entities would need to participate: Sonoma County Bicycle Coalition, SCTA, Sonoma County Office of Education, Safe Kids Sonoma County, Sonoma County Department of Health Services, Cal SERVES, Healthy Eating Active Living, Health Action, Healthy By Design, local schools and school districts, law enforcement agencies, service organizations, parent groups, local businesses, local bike clubs and teams, and volunteers.

### DISCUSS IMPLEMENTATION

Sonoma County is fortunate to have two successful SRTS projects to use as models. This program would be implemented by the Sonoma County Department of Health Services and the Sonoma County Bicycle Coalition with various partners using their expertise. Students will learn walking and bicycling safety through an established curriculum and will receive support and encouragement through events such as Walk and Roll to School days. An engineering firm will arrange walking audits and engineer capital improvement plans around school areas.

### TIMEFRAME

School year, on going

### BARRIERS TO SUCCESS

Funding, school participation, incomplete streets; missing sidewalks, excessive auto speeds, no bicycle lanes or paths for pedestrians and bicyclists.

### BENEFICIARIES

School-aged children, parents, school neighbors, local businesses.

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## SOLUTION C: COMPLETE THE CENTRAL SONOMA VALLEY BIKEWAY (CLASS I, MULTI-USE PATH)

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### PROBLEM DEFINITION

Bicycle and walking routes are needed for employees of Sonoma Mission Inn and Spa and other local employers, as well as for area students to access schools and recreation. CBTP outreach called for the expansion of the multi-use trail system. For additional supporting data, see "Final Report Central Sonoma Valley Bikeway Plan" prepared by Wilbur Smith Associates, 2M Associates in November 15, 2001.

### DESCRIPTION

Sonoma County's Regional Parks and Transportation & Public Works departments will work together on developing the Central Sonoma Valley Bikeway (Class I, II, and III). The Regional Parks Department will take the lead on Class I segments at the following locations:

- Verano Avenue - Crosswalk (from Verano Ave to Maxwell Park) to Highway 12
- West side of Highway 12 - Main Street to Encinas Lane
- Private Lot - End of Encinas Lane to private lot on Fairview Lane
- Private Lot on Fairview Lane - Encinas Lot to Fairview Lane
- West Thomson Ave - Melody Lane to Happy Lane
- Happy Lane - Private lot to Orchard Avenue
- Larson Park - Larson Park Entrance - Vailetti Drive

### IDENTIFY NEEDED RESOURCES:

- Estimated cost: Between \$1.9 (minimum improvements) and \$2.85 million (maximum improvements). Estimate is based on 2005 dollars.

- Potential funding sources: Transportation Development Act Article 3, Measure M, Proposition 40 Per Capita (expires 6/30/11), Park Mitigation Fees, State Local Partnership Program (avail. FY 10/11); Lifeline Transportation Program
- Lead & participating entities: Regional Parks will lead on Class I segments. Transportation and Public Works will lead on Class II and III segments.

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

The various segments will be completed in phases as funding becomes available. The Regional Parks Department is currently working on the Class I segment between Larson Park and Vailetti Drive.

#### BARRIERS TO SUCCESS

Property owners unwilling to grant public access easements on private land. Sonoma Valley Unified School District needs to grant "public" access easement on Flowery Elementary School property.

#### BENEFICIARIES

Pedestrians, bicyclists, students, employees of Sonoma Mission Inn and Spa and other local businesses, residents, tourists.

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### SOLUTION D: BICYCLE EDUCATION CAMPAIGN & STREET SKILLS CLASSES

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#### PROBLEM DEFINITION

Field observations revealed that many bicyclists in the study area would benefit from a greater understanding of how they could increase their personal safety while bicycling.

#### DESCRIPTION

Implement an educational campaign to reach out to bicyclists of all ages, including those in the Latino community, to raise awareness about safety practices such as direction of travel, safe turning movements, utilizing reflective protections and lights at night, helmet use, and bicycle maintenance. Use various methods to reach target audience: workshops, media, pamphlets and skills classes.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$25,000 (per year)
- Potential funding sources: Office of Traffic Safety, Lifeline Transportation Program, Measure M, donations; foundation grants, local businesses, Bikes Belong grants.

- What entities would need to participate: Sonoma County Bicycle Coalition, law enforcement, Sonoma County Transit, local bicycle and other businesses, volunteers/civic groups, community based organizations (e.g., La Luz Center)

### DISCUSS IMPLEMENTATION

Need for on-going program to reach different people over time. Since Sonoma County Transit has run a similar campaign, the program structure and materials are already created. The program includes four Bicycle Street Skills classes. This program will provide training for League Certified Instructors who speak both Spanish and English. It will create and provide bi-lingual materials and classes.

#### TIMEFRAME

Little time would be needed after resources secured. Campaign would roll out in phases, and classes would be quarterly.

#### BARRIERS TO SUCCESS

Funding, disinterest in participating by target group, availability of bi-lingual instructors.

#### BENEFICIARIES

Bicyclists (especially those unaware of safety practices), pedestrians, and motorists.

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### SOLUTION E: MAINTAIN EXISTING LEVELS OF TRANSIT SERVICE IN THE SPRINGS STUDY AREA

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#### PROBLEM DEFINITION

In recent years, transit funding has been impacted by various shortfalls. Further cuts in transit funding could lead to the necessity of making service cuts and/or fare increases.

While transit service enhancements are desirable in The Springs, it must be recognized that service enhancements are possible only if existing levels of transit service can be maintained.

#### DESCRIPTION

Provide adequate funding for transit operations to maintain existing levels of transit service in the study area and avoid service reductions and/or fare increases.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: Undetermined. Amount necessary would be based on potential additional reductions in the availability of funds for bus and paratransit operations.

- Potential funding sources: Transportation Development Act, Measure M, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit

## DISCUSS IMPLEMENTATION

### TIMEFRAME

Immediately, as existing service levels would be maintained.

### BARRIERS TO SUCCESS

Lack of funding.

### BENEFICIARIES

Bus and paratransit passengers traveling in and beyond the study area.

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## SOLUTION F: ENHANCE PEDESTRIAN CROSSINGS ON HIGHWAY 12 AT VARIOUS LOCATIONS

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### PROBLEM DEFINITION

Highway 12 is a major route in Sonoma County connecting Sonoma Valley to Santa Rosa and US 101. Highway 12 also functions as 'Main Street' within the community of The Springs. Though traffic speeds are lower through The Springs (25 to 30 mph posted), traffic volumes are very heavy throughout most of the day. When crossing Highway 12 by foot at anywhere but at one of three signalized intersections, it is difficult to find a break in the traffic to safely and comfortably complete the crossing. There is a recommendation to add pedestrian crossings on Highway 12 at the intersections of Donald Street, Thomson Avenue, Arroyo Road and Calle Del Monte, Central Avenue, and at the Sonoma Charter School; and also to enhance existing crossings.

### DESCRIPTION

Adding or enhancing a crossing may be as simple as installing signs and painting (or repainting) crosswalk markings, or may be more complex with installation of flashing warning lights/beacons to alert motorists of pedestrians. In either case it will be necessary to obtain Caltrans approval for any work done on Highway 12.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: depending on type of crossing - as little as \$5,000 each for signs/thermoplastic markings (\$25,000 for

five) to \$100,000 or more each for lights/beacon crossings (\$500,000 for five)

- Potential funding sources: Transportation Development Act Article 3, Sonoma County Community Development Commission, Lifeline Transportation Program.
- Lead & participating entities: Caltrans, Sonoma County Transportation and Public Works, various bicycle/pedestrian groups

## DISCUSS IMPLEMENTATION

### TIMEFRAME

One to two years

### BARRIERS TO SUCCESS

Adding pedestrian crossings to a heavily traveled roadway must be implemented with caution. Crosswalks may give pedestrians, especially children, a false sense of security, causing them to walk in front of motorists not expecting them. More pedestrian crossings mean more delay to traffic and more congestion within The Springs.

### BENEFICIARIES

Pedestrians crossing Highway 12

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## SOLUTION G: INSTALL MORE SHELTERS, BENCHES & BIKE RACKS AT BUS STOPS

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### PROBLEM DEFINITION

There are currently a limited number of benches, shelters and bike racks located at bus stops in the study area. While there are currently thirty Sonoma County Transit bus stops located along the Highway 12 corridor within The Springs area served by local and intercity bus routes, there are covered passenger waiting shelters or benches provided at ten of these bus stops. There are currently no bike racks located at bus stops in the study area.

### DESCRIPTION

Sonoma County Transit will install new passenger waiting shelters, benches and/or bike racks at its bus stops within The Springs area upon request, where feasible. Several factors are involved in determining the feasibility of installing a shelter, bench or bike rack at a bus stop. Most often, a bus stop cannot accommodate a shelter, bench or bike rack due to right-of-way limitations, incompatibility with nearby land-uses, and/or various other safety issues. Sonoma County Transit budgets federal and state funding to purchase, install and maintain new

shelters and benches throughout its service area on an annual basis. New bike racks can be acquired through regional air district grants. Redevelopment funding through the Sonoma County Community Development Commission funding may also be available to purchase and install new shelters and benches along the Highway 12 corridor.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$45,000 one-time cost (in 2010 dollars...assumes up to ten new shelters and benches, and up to ten new bike racks, including installation).
- Potential funding sources: Transportation Development Act, Bicycle Facility Program, Bicycle Transportation Account, Sonoma County Community Development Commission, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, Sonoma County Community Development Commission, Caltrans.

#### DISCUSS IMPLEMENTATION

##### TIMEFRAME

One to three years.

##### BARRIERS TO SUCCESS

Lack of adequate right-of-way and inability to receive consent from local businesses and property owners.

##### BENEFICIARIES

Sonoma County Transit's passengers utilizing local and intercity bus routes in The Springs, including bicyclists.

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### SOLUTION H: EXPAND OUTREACH & CUSTOMER SERVICE EFFORTS TO POTENTIAL & EXISTING LATINO BUS PATRONS

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#### PROBLEM DEFINITION

While the majority of existing and potential bus patrons in The Springs area are Latino who predominantly speak Spanish, the number of bus drivers who speak Spanish and the amount of printed information regarding routes and fares in Spanish is limited. Sonoma County Transit's printed bus schedules already include some Spanish-language translations related to route schedules and cash fares. Besides Sonoma County Transit's website, which is available to be viewed in Spanish, general policy and bus pass sales information on printed bus schedules are not presented in Spanish. Also,

Sonoma County Transit's advertising to Latino and Spanish-speaking individuals is limited.

#### DESCRIPTION

General information regarding Sonoma County Transit's bus schedules can be translated into Spanish on all of its bus schedules. Bus pass sales information and order forms for Sonoma County Transit's bus service can also be translated into Spanish. The promotion of Sonoma County Transit's bus routes and bus pass sales can be directed toward Spanish-language media outlets as well as to various community organizations in the study area. In addition, while all of Sonoma County Transit's bus drivers receive Spanish language training on an annual basis, more emphasis can be placed on improving customer service to Spanish-speaking passengers.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$5,000 annual cost (2010 dollars...includes Spanish translation services, some additional printing costs and new Latino-focused advertising expenses).
- Potential funding sources: Transportation Development Act, Measure M, Bay Area Air Quality Management's District Transportation Fund for Clean Air, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, La Luz Center, St Leo's Catholic Church.

#### DISCUSS IMPLEMENTATION

##### TIMEFRAME

Six months to one year.

##### BARRIERS TO SUCCESS

No barriers can be identified. Costs can be absorbed into Sonoma County Transit's existing printing and marketing budgets.

##### BENEFICIARIES

Existing and potential Latino and Spanish-speaking patrons utilizing Sonoma County Transit's bus services, especially in the study area.

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### SOLUTION I: INCREASE FREQUENCY OF ROUTE 40 BUSES TO/FROM THE SPRINGS & PETALUMA, INCLUDING SATURDAY SERVICE

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#### PROBLEM DEFINITION

Frequency of service for route 40 during weekdays is limited. Route 40 does not currently operate on weekend days. Sonoma County

Transit's service frequency on route 40 during weekdays currently averages every two hours and forty-three minutes. There is currently no route 40 service provided on weekend days.

### DESCRIPTION

Increasing the frequency of service (decreasing headways) on Sonoma County Transit's route 40 between The Springs and Petaluma to every sixty minutes on weekdays and the introduction of new route 40 service on Saturdays with two hour headways, for example, would require a substantial amount of additional funding. As an alternative to additional funding, bus routes in other parts of Sonoma County Transit's service area could be reduced substantially or completely eliminated to accommodate increased frequencies on route 40. However, prior to any such changes, ridership counts and passenger surveys on routes throughout Sonoma County Transit's fixed-route bus system would need to be conducted and analyzed to determine how they might impact passengers.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$375,000 annual cost (2010 dollars...assumes 60% increase in existing route 40 weekday service hours and the introduction of new route 40 service on Saturdays).
- Potential funding sources: Transportation Development Act, Measure M, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, cities of Sonoma and Petaluma

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

Service could be gradually implemented over several years if an adequate and on-going funding source were to be secured.

#### BARRIERS TO SUCCESS

Lack of funding.

#### BENEFICIARIES

Route 40 passengers traveling between The Springs (via Sonoma) and Petaluma.

## SOLUTION J: INCREASE FREQUENCY OF ROUTE 30 BUSES TO/FROM THE SPRINGS & SANTA ROSA & SONOMA

### PROBLEM DEFINITION

Frequency of service for route 30 is limited, especially on weekend days. Sonoma County Transit's service frequency on route 30 during weekdays currently averages every one hour and twenty-three minutes and on weekend days averages every three hours and thirty minutes.

### DESCRIPTION

Increasing the frequency of service (decreasing headways) on Sonoma County Transit's route 30 between The Springs and Santa Rosa and between The Springs and Sonoma to every forty-five minutes on weekdays and to every one hour and thirty minutes on weekend days, for example, would require a substantial amount of additional funding. As an alternative to additional funding, bus routes in other parts of Sonoma County Transit's service area could be reduced substantially or completely eliminated to accommodate increased frequencies on route 30. Prior to any such changes, however, ridership counts and passenger surveys on routes throughout Sonoma County Transit's fixed-route bus system would need to be conducted and analyzed to determine how they might impact passengers.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$600,000 annual cost (2010 dollars...assumes 50% increase in existing route 30 weekday and weekend service hours).
- Potential funding sources: Transportation Development Act, Measure M, Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, city of Sonoma.

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

Service could be gradually implemented over several years if an adequate and on-going funding source were to be secured.

#### BARRIERS TO SUCCESS

Lack of funding.

#### BENEFICIARIES

Route 30 passengers traveling between The Springs and Santa Rosa, and between The Springs and Sonoma.



## **SOLUTION K: COMPLETE VERANO AVENUE SIDEWALKS FROM HIGHWAY 12 TO SONOMA CREEK**

### **PROBLEM DEFINITION**

There are existing sidewalks on Verano Avenue from Arnold Drive to Sonoma Creek. There is a gap in sidewalks from Sonoma Creek to Highway 12. Likewise there is a gap in the Class II bicycle lanes over the adjacent bridge, however a walkway is present. Verano Avenue passes through a partially urbanized, partially rural area between Highway 12 and Sonoma Creek. The rural segment has a canopy of trees growing within a few feet of the edge of pavement. Construction of curb, gutter and sidewalk will require removal of a significant number of trees. Adding curb and gutter to a roadway typically triggers addition or modification of underground storm drains to account for changes to surface storm water runoff caused by curb and gutter.

### **DESCRIPTION**

Design and construct sidewalk and drainage improvements along Verano Avenue.

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost: \$600,000
- Potential funding sources: Transportation Development Act Article 3, Lifeline Transportation Program, Sonoma County Community Development Commission
- Lead & participating entities: Sonoma County Transportation and Public Works,

### **DISCUSS IMPLEMENTATION**

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

### **TIMEFRAME**

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six to eight months to construct.

### **BARRIERS TO SUCCESS**

Widening an existing roadway to construct sidewalks within an urban corridor often results in impacts to frontage of properties (mostly residential) and removal of parking for vehicles. Past projects in the area have met with a strong opposition to removing trees, especially native oaks.

### **BENEFICIARIES**

Pedestrians, especially children, who walk along Verano Avenue.

## **SOLUTION L: ARNOLD DRIVE BICYCLE LANES FROM AGUA CALIENTE ROAD TO COUNTRY CLUB DRIVE**

### **PROBLEM DEFINITION**

The existing road shoulders are narrow (typically two feet wide) and do not provide adequate width to comfortably accommodate bicycles.

### **DESCRIPTION**

Design and construct widening of existing roadway to provide a minimum six foot wide Class II bike lane. It may be possible to construct intermittent segments of Class I bike lane separated from the existing roadway to avoid tree removal.

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost : \$1,500,000
- Potential funding sources: Measure M, Transportation Development Act Article 3, Regional Bicycle Program, Bicycle Facility Program, Bicycle Transportation Account, Lifeline Transportation Program
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle groups, SCTA

### **DISCUSS IMPLEMENTATION**

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

### **TIMEFRAME**

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six to eight months to construct.

### **BARRIERS TO SUCCESS**

This segment of Arnold Drive has a canopy of trees, mostly native oaks, overhanging the roadway. The trees grow within several feet of the existing paved shoulder. There has been very strong local opposition to any roadway improvements that cause removal of trees.

## BENEFICIARIES

Bicyclists, pedestrians, especially school children, bicycle commuters, and recreational bicyclists.

### SOLUTION M: IMPLEMENT A NEW WEEKDAY BUS ROUTE BETWEEN THE CITIES OF SONOMA & NAPA

#### PROBLEM DEFINITION

There is currently no public bus service or Americans with Disabilities Act (ADA) paratransit service provided between the cities of Sonoma and Napa, respectively. Several key destinations and origins between the city of Sonoma and the city of Napa are not currently served by public transit. In Sonoma County, this includes several destinations along the south side of Broadway and along Highway 121 between Arnold Drive and the Sonoma/Napa county line. ADA paratransit service is also not available for eligible persons with disabilities in these areas of Sonoma County because there is currently no associated fixed-route transit in these areas.

#### DESCRIPTION

Providing new public transit service between the city of Sonoma and city of Napa would require additional funding, as well as a funding and operating agreement between the County of Sonoma and the County of Napa. As an alternative to additional funding from the County of Sonoma, bus routes in other parts of Sonoma County Transit's service area could be reduced substantially or completely eliminated. However, prior to any such changes, ridership counts and passenger surveys on routes throughout Sonoma County Transit's fixed-route bus system would need to be conducted and analyzed to determine how they might impact passengers.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$175,000 annual cost (2010 dollars...assumes five eastbound trips and 5 westbound trips per weekday providing new service between the cities of Sonoma and Napa, as well as expanded comparable ADA paratransit service).
- Potential funding sources: Transportation Development Act, Measure M, and/or Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, city of Sonoma, city of Napa, Napa County.

## DISCUSS IMPLEMENTATION

### TIMEFRAME

Service could be implemented within one year if an adequate and on-going funding source was secured, and if an operating agreement was approved between the counties of Sonoma and Napa.

### BARRIERS TO SUCCESS

Lack of funding.

### BENEFICIARIES

Residents, businesses, and other organizations desiring bus and paratransit service between Sonoma Valley and Napa.

### SOLUTION N: AGUA CALIENTE ROAD BICYCLE LANES FROM HIGHWAY 12 TO ARNOLD DRIVE

#### PROBLEM DEFINITION

The existing road shoulders are narrow (typically two feet wide) and do not provide adequate width to comfortably accommodate bicycles.

#### DESCRIPTION

Design and construct widening to existing roadway to provide a minimum six foot wide Class II bike lane.

#### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$2,500,000
- Potential funding sources: Transportation Development Act Article 3, Bicycle Facility Program, Bicycle Transportation Account, Lifeline Transportation Program
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle groups

#### DISCUSS IMPLEMENTATION

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

### TIMEFRAME

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six months to construct.

### BARRIERS TO SUCCESS

A segment of Agua Caliente Road within this project has a canopy of trees, mostly native oaks, over-

hanging the roadway. Trees grow within several feet of the existing paved shoulder. There has been very strong local opposition to any roadway improvements that cause removal of trees. There is also a bridge located within the project limits that is not scheduled for replacement in the near future. This bridge doesn't have sufficient width (approximately 28 feet) to allow Class II Bike Lanes.

### **BENEFICIARIES**

Bicyclists, pedestrians, bicycle commuters, and recreational bicyclists.

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## **SOLUTION O: BOYES BOULEVARD SIDEWALKS FROM HIGHWAY 12 TO ARNOLD DRIVE**

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### **PROBLEM DEFINITION**

Boyes Boulevard passes through an urbanized area within the project limits. There are existing intermittent sidewalks. Much of the existing sidewalk likely needs upgrades to meet current accessibility standards. Adding curb and gutter to a roadway typically triggers addition or modification of underground storm drains to account for changes to surface storm water runoff caused by curb and gutter. There is an existing bridge on Boyes Blvd over Sonoma Creek that currently does not have accessible sidewalks. This bridge is scheduled to be replaced in the County's Five-year Capital Project Plan.

### **DESCRIPTION**

Design and construct sidewalk and drainage improvements along Boyes Boulevard.

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost: \$800,000 depending on widening
- Potential funding sources: Transportation Development Act Article 3, Sonoma County Community Development Commission, Lifeline Transportation Program
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle groups

### **DISCUSS IMPLEMENTATION**

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

### **TIMEFRAME**

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six to eight months to construct.

### **BARRIERS TO SUCCESS**

Widening an existing roadway to construct sidewalks within an urban corridor often results in impacts to frontage of properties (mostly residential) and removal of parking for vehicles. There is also a bridge located within the project limits that is scheduled for replacement in the near future. The existing bridge doesn't have sufficient width (approximately 28 feet) to allow Class II Bike Lanes.

### **BENEFICIARIES**

Pedestrians, especially children, who walk along Boyes Boulevard.

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## **SOLUTION P: BOYES BOULEVARD BICYCLE LANES FROM HIGHWAY 12 TO ARNOLD DRIVE**

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### **PROBLEM DEFINITION**

Boyes Boulevard passes through an urbanized area within the project limits. Existing roadway has paved shoulders varying in width. Existing road shoulders with adequate widths are likely used for vehicle parking. The project would require widening the road shoulder pavement to a minimum of six feet and signing and striping to prevent vehicles from parking in the bike lane. There is an existing bridge on Boyes Boulevard over Sonoma Creek that currently does not have the desired shoulder width for a Class II bike lane. This bridge is scheduled to be replaced in the County's Five-year Capital Project Plan.

### **DESCRIPTION**

Design and construct widening to existing roadway to provide a minimum six foot wide Class II bike lane.

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost: \$1,500,000 to \$3,000,000 depending on widening
- Potential funding sources: Transportation Development Act Article 3, Bicycle Facility Program, Bicycle Transportation Account, Lifeline Transportation Program
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle groups

## DISCUSS IMPLEMENTATION

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

### TIMEFRAME

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six to eight months to construct.

### BARRIERS TO SUCCESS

Widening a roadway within an urban corridor often results in impacts to frontage of properties (mostly residential) and removal of parking for vehicles. An alternative would be to widen the roadway sufficiently to allow for continued on street parking. There is also a bridge located within the project limits that is scheduled for replacement in the near future. The existing bridge doesn't have sufficient width (approximately 28 feet) to allow Class II Bike Lanes

### BENEFICIARIES

Bicyclists, pedestrians, bicycle commuters, and recreational bicyclists.

## SOLUTION Q: LATER AFTERNOON AND/OR EVENING BUS SERVICE AND EXPAND ADA PARATRANSIT SERVICE BETWEEN THE SPRINGS AND THE CITIES OF SONOMA, SANTA ROSA AND PETALUMA, RESPECTIVELY

### PROBLEM DEFINITION

Bus and Americans with Disabilities Act (ADA) paratransit services do not run late enough into the afternoon/evening in The Springs area. The hours that bus and ADA paratransit services currently operate within The Springs area is limited, when compared with more urbanized transit service. Intercity route 30 operates during weekdays within The Springs area between 5:50 a.m. and 9:15 p.m. During weekend days, route 30 provides service within the area between 8:30 a.m. and 7:45 p.m. Intercity route 40 operates during weekdays within the area between 7:00 a.m. and 6:00 p.m. Local route 32 provides weekday service within The Springs area between 7:45 a.m. and 5:00 p.m. ADA paratransit service is also available for eligible persons with disabilities during these hours within route 30, route 32 and route 40 service areas.

## DESCRIPTION

Expanding the evening hours of bus and ADA paratransit service on Sonoma County Transit's route 30, route 32 and route 40 in The Springs area would require additional funding. As an alternative to additional funding, the expansion of evening service could potentially be accomplished with a comparable reduction to route 30, route 32 and route 40 service hours (as well as ADA paratransit service hours) in the morning or mid-day. However, prior to such changes, ridership counts and passenger surveys on route 30, route 32 and route 40 would need to be conducted and analyzed to determine how they might impact passengers utilizing these routes (and/or ADA paratransit services) in the morning or mid-day.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$275,000 annual cost (2010 dollars...assumes one additional weekday and weekend route 30 eastbound evening trip, one additional weekday and weekend route 30 westbound evening trip, one additional weekday route 40 eastbound evening trip, one additional weekday route 40 westbound evening trip, three additional weekday and Saturday route 32 afternoon/evening trips serving existing areas, as well as expanded comparable ADA paratransit service).
- Potential funding sources: Transportation Development Act, Measure M, and/or Lifeline Transportation Program.
- Lead & participating entities: Sonoma County Transit, cities of Sonoma and Petaluma

## DISCUSS IMPLEMENTATION

### TIMEFRAME

Service could be gradually implemented over several years if an adequate and on-going funding source were to be secured.

### BARRIERS TO SUCCESS

Lack of funding.

### BENEFICIARIES

Route 30, 32 and 40 passengers traveling between The Springs and the cities of Sonoma, Santa Rosa and Petaluma, respectively.

## **SOLUTION R: PEDESTRIAN LIGHTING ON HIGHWAY 12 FROM DONALD STREET TO VERANO AVENUE**

### **PROBLEM DEFINITION**

There is a lack of street lighting along segments of roads that have been identified as heavily used by pedestrians. A segment of Highway 12 between the Fruit Basket at West Thomson Ave and Verano Avenue has been identified as one of these segments.

### **DESCRIPTION**

There is intersection lighting and ornamental pedestrian lighting on each side of this section of Highway 12 between West Thomson and Encinas Lane. There is a gap in pedestrian lighting for about 500 feet south of Encinas Lane on the west side of Highway 12. There is gap in lighting on the east side of Highway 12 from Encinas Lane to Verano Avenue. There is intersection lighting at Donald Street and Harley Street. Installation of uniform pedestrian lighting, perhaps ornamental type, would most likely occur in conjunction with other road improvements such as a sidewalk project. Sonoma County Transportation and Public Works and Community Development Commission are currently working with Caltrans to develop a project that would install sidewalks and pedestrian lighting between Donald Street and Verano Avenue

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost: \$150,000
- Potential funding sources: Valley of the Moon Lighting District, Sonoma County Community Development Commission
- Lead & participating entities: Sonoma County Transportation and Public Works, Community Development Commission

### **DISCUSS IMPLEMENTATION**

#### **TIMEFRAME**

Two to three years

#### **BARRIERS TO SUCCESS**

Because the project is within the state highway right-of-way all improvements must be approved by Caltrans. The process to work through design and acceptance of plans is lengthy. There could be localized opposition to installing lighting from residents that will be disturbed by bright lights at night.

### **BENEFICIARIES**

Pedestrians and bicyclists who use the sidewalks/roadways at night.

## **SOLUTION S: AGUA CALIENTE ROAD SIDEWALKS FROM HIGHWAY 12 TO VAILETTI DRIVE**

### **PROBLEM DEFINITION**

Agua Caliente Road passes through an urbanized area within the project limits. There are intermittent sidewalks along this roadway segment, however, much of the existing sidewalks likely need upgrades to meet current accessibility standards. Adding curb and gutter to a roadway typically triggers addition or modification of underground storm drains to account for changes to surface storm water runoff caused by curb and gutter. There is an existing bridge on Agua Caliente Road to the west of Vailetti Drive over Sonoma Creek that currently does not have accessible sidewalks, or sufficient width to add bicycle lanes. This bridge is not scheduled to be replaced in the near future.

### **DESCRIPTION**

Design and construct sidewalk and drainage improvements along Agua Caliente Road from Highway 12 to Vailetti Drive.

### **IDENTIFY NEEDED RESOURCES**

- Estimated cost: \$600,000
- Potential funding sources: Transportation Development Act Article 3, Lifeline Transportation Program
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle groups

### **DISCUSS IMPLEMENTATION**

Before undertaking this project it will be necessary to have funding sources in place and sufficient support of local leaders/government officials and bicycle/pedestrian community to offset expected opposition.

#### **TIMEFRAME**

Design, environmental, right-of-way and utility relocation required in advance of construction will likely take four to five years. Once approved, six to eight months to construct.

#### **BARRIERS TO SUCCESS**

Widening an existing roadway to construct sidewalks within an urban corridor often results in



impacts to frontage of properties (mostly residential) and removal of parking for vehicles.

### BENEFICIARIES

Pedestrians, especially children, who walk along Agua Caliente Road.

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## SOLUTION T: PROVIDE INCENTIVES FOR BUSINESSES TO PROVIDE SAFE & CONVENIENT BICYCLE PARKING

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### PROBLEM DEFINITION

There is a lack of safe and convenient bicycle parking at businesses in The Springs. This is a barrier to greater utilization of bicycles for travel in the study area.

According to a recent community outreach survey conducted in The Springs area, almost 38% of survey respondents indicated that they don't drive a car. Instead, a significant proportion of survey respondents walk, bicycle, carpool and/or use public transit as their primary modes of transportation. Of the survey respondents who indicated that they ride their bicycles to work, over 43% stated that there were no bicycle parking facilities at their destinations.

### DESCRIPTION

A new Bicycle Rack Incentive Program for The Springs area can be established by the Sonoma County Community Development Commission and/or Sonoma County Transportation and Public Works Department to encourage installation of new bicycle racks on private property adjacent to local businesses. Under such a program, County staff would meet with interested property and business owners to determine the number of bicycles to be accommodated and an appropriate location for a bicycle rack at the place of business. The County would then purchase and install the bicycle rack at no cost to property owners or businesses, with the understanding that the bicycle rack would then become their property and responsibility.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: \$20,000 one-time cost (2010 dollars...assumes up to 50 bicycle racks, including installation).
- Potential funding sources: Transportation Development Act Article 3, Bay Area Air Quality Management District, Community Development Commission, Bicycle Transportation Account

- Lead & participating entities: Community Development Commission, Sonoma County Transportation and Public Works.

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

One to two years.

#### BARRIERS TO SUCCESS

Lack of adequate right-of-way, inability to receive consent from local businesses and property owners.

### BENEFICIARIES

Bicyclists and businesses/property owners in the study area.

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## SOLUTION U: REINSTITUTE GOLDEN GATE TRANSIT ROUTE 90 BUS SERVICE FROM SONOMA VALLEY TO SAN RAFAEL & SAN FRANCISCO

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### PROBLEM DEFINITION

There is a need for improved interregional connectivity between the study area and Marin County and San Francisco.

### DESCRIPTION

Service would consist of five trips:

One early morning southbound commute trip to San Francisco

One late morning southbound trip to San Francisco

One morning northbound trip from San Francisco (this trip became the late morning southbound trip)

One northbound evening commute trip from San Francisco

One "short" trip returning from Sonoma to Novato (where our bus yard is located) that passengers could use to connect to other service destined for San Francisco.

The early morning southbound trip from Sonoma would originate at the Novato bus yard and operate as a deadhead trip to Sonoma. All of the above service would operate weekdays only.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: For FY 10/11, Weekday service would cost approximately \$440,000 per year; Saturday service would cost approximately \$67,659 per year.

- Potential funding sources: Measure M, Transportation Development Act, Lifeline Transportation Program
- Lead & participating entities: Golden Gate Transit, SCTA

### DISCUSS IMPLEMENTATION

Some fare revenue would be expected to offset operating costs. The following is a rough estimate of what revenues could be under a scenario where 20 passengers ride each trip, and assuming half were going to/from San Rafael and half to/from San Francisco (this is a typical benchmark: Golden Gate Transit service against a performance standard of 20 passengers per trip or a half-full 40-passenger bus). Under this scenario, approximately \$153,400 in annual weekday revenue might be generated to offset the operating cost of \$440,000. If more than twenty passengers per trip could be generated or more passengers were going to San Francisco, the revenue offset could be higher. For Saturday service, the same revenue assumptions would generate approximately \$31,400 per year.

### TIMEFRAME

Service could be implemented soon after all approvals are obtained and after adequate and on-going funding source(s) are secured.

### BARRIERS TO SUCCESS

Lack of funding. Reinstating route 90 service would require a large amount of outside subsidy. This route was discontinued in 2003 because it did not do well in ridership relative to the amount of subsidy that it required.

### BENEFICIARIES

Sonoma Valley residents who need to travel to Marin or San Francisco for work, medical appointments, or out of region travel and people who need or want to travel from San Francisco or Marin to Sonoma Valley for work or tourism.

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## SOLUTION V: ADD PEDESTRIAN CROSSINGS ON VERANO AVENUE AT RIVERSIDE DRIVE

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### PROBLEM DEFINITION

Safety is a concern for those crossing Verano Avenue on foot at this intersection.

### DESCRIPTION

Adding a crossing may be as simple as installing signs and painting crosswalk markings, or may be

more complex with installation of flashing warning lights/beacons to alert motorists of pedestrians.

### IDENTIFY NEEDED RESOURCES

- Estimated cost: Depending on type of crossing – as little as \$5,000 each for signs/thermoplastic markings to \$100,000 or more each for lights/beacon crossings
- Potential funding sources: Transportation Development Act Article 3
- Lead & participating entities: Sonoma County Transportation and Public Works, various bicycle/pedestrian groups

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

One to two years

#### BARRIERS TO SUCCESS

Adding pedestrian crossings to heavily traveled roadways must be done with caution. Crosswalks may give pedestrians, especially children, a false sense of security, causing them to walk in front of motorists not expecting them.

#### BENEFICIARIES

Pedestrians crossing Verano Avenue

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## SOLUTION W: PEDESTRIAN LIGHTING AGUA CALIENTE ROAD & BOYES BOULEVARD

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### PROBLEM DEFINITION

There is a lack of street lighting along segments of roads that have been identified as heavily used by pedestrians. County roads specifically mentioned are Agua Caliente Road between Highway 12 and Vailletti Drive, and Boyes Boulevard between Highway 12 and Arnold Drive.

### DESCRIPTION

There is some existing street lighting on these sections of roadway. These lights are mounted on power poles and located mostly at intersections. Additional lighting could be installed between intersections on existing utility poles. A request could be made to the Valley of the Moon Lighting District for this. The district may have sufficient funding to add these types of lights.

Installation of uniform pedestrian lighting, perhaps ornamental type, would most likely

occur in conjunction with other road improvements such as a sidewalk project.

### IDENTIFY NEEDED RESOURCES

- Estimated cost Agua Caliente: \$300,000; Boyes - \$600,000
- Potential funding sources: Valley of the Moon Lighting District, Sonoma County Community Development Commission
- Lead & participating entities: Sonoma County Transportation and Public Works, Sonoma County Community Development Commission

### DISCUSS IMPLEMENTATION

#### TIMEFRAME

Will depend on scale of project(s). Could be as little as a half-year after funding is secured and the projects are approved.

#### BARRIERS TO SUCCESS

For ornamental lighting the cost will be the biggest obstacle. It may be possible to add infill lighting on existing utility poles with minor amount of funding outside the lighting district. There could be localized opposition to installing lighting from residents that will be disturbed by bright lights at night.

#### BENEFICIARIES

Pedestrians and bicyclists who use the sidewalks/roadways at night.

Two of the identified solutions are already being addressed, and were therefore removed from the ranked list. The first is intersection improvements at Arnold Drive and Agua Caliente Road. The intersection at Arnold Drive and Agua Caliente currently operates as all stop control. At times the traffic backs up for several hundred feet in each direction. There is a need to improve traffic flow and reduce delay. The Sonoma County Transportation and Public Works is currently developing improvement plans that will include installation of a traffic signal or construction of a roundabout. Either of these improvements will improve traffic flow through this intersection and reduce delay. Design and environmental documents must be completed and a minor amount of right-of-way acquired. The estimated cost for the improvements ranges from \$600,000 to \$700,000. Funding is identified through Measure

M and Proposition 1 B, with construction estimated to occur in one to two years.

The second solution called for signal coordination of Highway 12. There are perceived problems with traffic flow related to the existing signal coordination and timing along Highway 12. The signals along Highway 12 are owned, operated and maintained by Caltrans. Recent improvements to Highway 12 that constructed sidewalks and overlayed the roadway between Donald Street and Boyes Boulevard also included repairs/upgrades to signal hardware at three signalized intersections. Further repairs/upgrades will be made with construction of a second stage of sidewalks between Boyes Boulevard and Agua Caliente Road. If these improvements do not result in sufficient increases in traffic efficiency, additional work might be done to improve the coordination between the signals along this section of Highway 12. Sonoma County Transportation and Public Works will work with Caltrans for any improvements, including some type of signal interconnection. Motorists traveling through The Springs along Highway 12 would benefit. There is also a desire by some to increase the number of pedestrian crossings along Highway 12, however, the interruption of traffic flow caused by pedestrian crossings would result in greater congestion.

### FUNDING SOURCES

The following lists some of the funding sources commonly used to implement transportation improvements. Included are potential sources for the proposed solutions detailed above.

### FEDERAL SOURCES

#### STP

Surface Transportation Program (STP) Transit Capital Shortfall funds are Federal Highway Administration funds that MTC region "flexes" to transit capital projects. One of the key funding programs in the Transportation Equity Act for the 21st Century (TEA 21,) STP moneys are "flexible," meaning they can

be spent on mass transit, pedestrian and bicycle facilities, and road and highway improvements..

### **TEA**

The Transportation Enhancements Activities (TEA) program offers communities the opportunity to expand transportation choices. Activities such as safe bicycle and pedestrian facilities, scenic routes, beautification, and other investments increase opportunities for recreation, accessibility, and safety for everyone beyond traditional highway programs. Ten percent of STP moneys must be set aside for projects that enhance the compatibility of transportation facilities with their surroundings.

### **CMAQ**

The Congestion Mitigation and Air Quality Improvement Program (CMAQ) provides funds for transportation projects that improve air quality. Eligible pedestrian and bicycle-related projects include transportation facilities, safety and education programs, and promotional programs. Other eligible uses include transit capital projects, such as acquisition of clean-fuel buses and operating expenses for new service. These funds are received for distribution by MTC.

### **FTA**

Federal Transit Administration (FTA) is one of the agencies of the U.S. Department of Transportation (USDOT)

### **FTA 5307/5309**

The 5307 program is a capital program based on urbanized area formulas (for such as replacement or expansion of buses or bus facilities) while the 5309 capital program is essentially congressional earmarks.

### **FTA 5311**

Rural: Funds are distributed to the regions on non-urbanized area formula. These funds are used for transit capital and operating purposes in non-urbanized areas. Possible source for funding bus shelters, benches, and signage.

### **FTA 5316**

Job Access and Reverse Commute (JARC) funds are directed to services that provide transportation to low-income individuals. MTC prioritizes JARC funds through the Lifeline Transportation Program. Access to jobs is the goal. Grants can fund capital and operating costs.

### **FTA 5317**

New Freedom Program funds are directed to elderly and disabled transportation services. The formula grant program seeks to reduce barriers to transportation services and expand transportation mobility options available to seniors and to people with disabilities beyond requirements of the Americans with Disabilities Act (ADA) of 1990.

### **RSTP**

The Regional Surface Transportation Program (RSTP) is a block grant program for roads, bridges, transit capital and bicycle and pedestrian projects, including bicycle and pedestrian facilities, activated traffic lights, pedestrian and bicycle trails.

### **HRRRP**

The High Risk Rural Roads Program (HRRRP) is a component of the federal Highway Safety Improvement Program (HSIP). The HRRRP supports road safety program efforts through the implementation of construction and operational improvements on high risk rural roads.

### **HES**

Administered by Caltrans, the Hazard Elimination Safety Program (HES) is a federal safety program that provides funds for safety improvements on all public roads and highways. These funds serve to eliminate or reduce the number and/or severity of traffic accidents at locations selected for improvement.

### **CDBG**

The Community Development Block Grant (CDBG) program is a flexible program administered by the U.S. Department of Housing and Urban Development (HUD) that provides communities with resources to address a wide range of unique community development needs.

## **STATE SOURCES**

### **TDA**

The Transportation Development Act (TDA) is a key source of transit operators' operating revenue. TDA funds are generated from a statewide 4-cent sales tax on all retail sales in each county. This funding, administered by MTC, is used for transit, special transit for disabled persons, and bicycle and pedestrian purposes. TDA can be used for capital and operational expenditures.

**TDA3**

Transportation Development Act, Article 3 (TDA3) is a set-aside of approximately 2% of those monies for bicycle and pedestrian planning and projects. MTC administers TDA3, which is distributed based on population. Sonoma County's cities/town and County of Sonoma may use this funding for bicycle lanes, bicycle and pedestrian paths, and related planning and marketing efforts.

**BTA**

The Bicycle Transportation Account (BTA) is administered by Caltrans. Funding is aimed at improvements in the safety and convenience of bicycle commuters. Jurisdictions must have an adopted and certified bicycle plan in place to qualify. Grants can be used for design, engineering and construction of bicycle lanes and paths, and supporting amenities.

**SHOPP**

Caltrans is responsible for maintaining and operating the State Highway System. Caltrans monitors the condition and operational effectiveness of highways through periodic inspection, traffic studies and system analysis, and then uses the information to prepare the Ten-Year State Highway Operation and Protection Plan. The State Highway Operation and Protection Program (SHOPP) is used to improve traffic safety; preserve bridges, roadways and roadsides; increase mobility; and improve highway-related facilities.

**OTS**

The California Office of Traffic Safety (OTS) Program funds projects to reduce the number of persons killed in traffic collisions, alcohol-involved collisions, hit and run fatal and injury collisions, and nighttime fatal and injury collisions. On an annual basis OTS requests proposals for projects from public agencies, including cities, school districts, and public safety providers.

**Prop 1B**

Proposition 1B (Infrastructure Bond) is a \$20 billion dollar general obligation bond measure passed by the voters in 2006, having various parts. One part makes funds available for rehabilitation, safety or modernization improvements, capital service enhancements or expansions, new capital projects, bus rapid transit improvements, or for rolling stock procurement, rehabilitation or replacement. Revenues are made available to transit operators for capital projects through MTC's Lifeline Transportation Program.

**SRTS/SR2S**

There are federal, state (Caltrans) and regional (MTC) safe routes to schools programs. Safe Routes to School Programs are intended to reverse the trend of dramatic decreases in the number of K-12 children walking and bicycling to school as compared to say thirty years ago. By funding projects that improve safety, and efforts that promote walking and bicycling within a collaborative community framework, children will be able to gain health benefits of greater physical exercise, and local air pollution and traffic congestion are reduced. The program involves working with coalitions of parents; school principals, teachers and other school staff; transportation professionals; law enforcement, and health care providers.

**REGIONAL SOURCES****Lifeline Transportation Program**

County Lifeline programs are established to fund projects that result in improved mobility for low-income residents. Lifeline can fund new or expanded services including: enhanced fixed route transit services, shuttles, children's programs, taxi voucher programs, improved access to autos, and capital improvement projects. Transportation needs specific to elderly and disabled residents of low-income communities may also be considered. MTC uses various funding sources to create this program. Projects must arise from a community planning process, such as The Springs Community Based Transportation Plan.

**RBP**

MTC's Regional Bicycle Program (RBP) is designed to fund construction of unbuilt segments the Regional Bicycle Network. The older Regional Bicycle Pedestrian Program (RBPP) has ended.

**TFCA**

The Bay Area Air Quality Management District (BAAQMD) awards Transportation Fund for Clean Air (TFCA) funding to projects known to provide a benefit to air quality. These funds are generated through a \$4 surcharge on vehicle registrations in the Bay Area. Each county is guaranteed 40% of the funds generated within their county for the TFCA Program Manager Program (administered by SCTA for Sonoma County). The remaining 60% of funds is administered by BAAQMD as the TFCA Regional Program.

**BFP**

The Bay Area Air Quality Management District's (Air District's) Bicycle Facility Program (BFP) provides grant funding to reduce motor vehicle emissions through implementation of new bikeways and bicycle parking facilities. BFP is funded through the Transportation Fund for Clean Air (TFCA) program. Eligible project types include: Class I - bicycle paths, Class II - bicycle lanes, Class III - bicycle routes, bicycle lockers and racks, secure bicycle parking, bicycle racks on public transportation vehicles.

**LOCAL SOURCES****The Springs Redevelopment Agency**

Redevelopment uses a dedication of part of the redevelopment area's property taxes to improve the health and safety conditions in the designated redevelopment area. Redevelopment focuses on eliminating "blighting conditions," a broadly defined term that can refer to physical, economic or social conditions. The preservation and expansion of employment and affordable housing opportunities are also goals. In The Springs, the infrastructure improvements on Highway 12 were deemed the highest priority. The recently completed and future Phase II renovations are being funded from this source. It is possible that funding for bus stop shelters and benches, signage, bicycle paths, additional sidewalks and bicycle amenities could be considered by RAC/CDC. See more about the Community Development Commission (CDC) and the Redevelopment Advisory Committee (RAC) in Chapter 2.

**Measure M**

Passed by the voters in November 2004, the Traffic Relief Act for Sonoma County (Measure M) provides for a  $\frac{1}{2}$  cent sales tax to be used to maintain local streets, fix potholes, accelerate widening Highway 101, improve interchanges, restore and enhance transit, support development of passenger rail, and build safe bicycle and pedestrian facilities. Funds are dedicated to specific programs and projects specified in the measure's Expenditure Plan. SCTA administers the sales tax distribution and prepares Measure M Strategic Plans. Revenues are allocated as follows: 40% to local street projects; 40% to Highway 101 improvements; 10% to transit services; 5% to the Sonoma Marin Area Rail Transit (SMART) train project; and 4% to bicycle and pedestrian projects.

**Regional Park Mitigation Fees**

Sonoma County Regional Parks receives park mitigation fees from certain types of residential development. These fees can be applied to specific types of park and trail planning and development projects.

**County Capital Budgets**

Many of the funding sources listed, plus others, may be folded into the County's Capital Budget. The Capital Budget can be used to fund infrastructure improvements, such as roads, bicycle and pedestrian facilities, and bus shelters.

**SCAPOSD**

The Sonoma County Agricultural Preservation and Open Space District (SCAPOSD) was established by Measure A. Approved by the voters in 1990, it is funded by a  $\frac{1}{2}$  cent sales tax approved by the voters through Measure C. SCAPOSD acquires properties and property easements for development and use as trails and regional parks.

**Developer Fees & Mitigations**

When projects move through the permitting process, there may be opportunities to condition projects to build infrastructure such as sidewalks, bicycle lanes and transit amenities; or to contribute impact fees for transit/transportation improvements.

**County Traffic Mitigation Fees**

County Traffic Mitigation Fees are one such example of the development fees described above.

**Volunteers/ Civic Groups/  
Donations/ Fund Raisers**

Volunteer efforts can often fill gaps in governmental and business-provided services. A prime example in the study area is the role volunteer drivers play in providing rides to seniors—giving of their time, car use, and gasoline. Volunteers are also partners in the Safe Routes to School programs. Civic groups, such as Rotary Clubs and Kiwanis, made up of volunteers, may also contribute to transportation-related solutions. Private or group donations and money gathered through such methods as raffles and fund raisers could also contribute to transportation-related solutions and supports.

**Local Businesses and Employers**

Local business entities and employers can play a role in improving transportation choices in an area. Businesses, for example, can participate in the installation of sidewalks; offer their employees transit



passes, or provide shuttle services. Many times local businesses are also contributors to civic programs. An example is Safe Routes to School (e.g., contributions of items for use as incentives). Businesses may also install bicycle and pedestrian amenities, such as benches and bicycle racks/lockers. As a prime study area example, the Fairmont Sonoma Mission Inn and Spa provides bicycle parking and promotes car-free commuting with incentives on “Don’t Drive to Work” days the first Wednesday of each month.

## OTHER SOURCES

### Foundations, Non-Profits

National and local non-profit organizations and private foundations can also be potential sources of funding. An example might be support of Safe Routes to Schools efforts, or a gift for beautification initiatives. Example foundations are: Community Foundation of Sonoma County, Robert Woods Johnson Foundation (promoting health through physical activity), Surdna Foundation (community revitalization), and the William G. Irwin Charity Foundation (capital grants can be used for bus shelters, shuttle vans, bus benches).

### Bikes Belong

Based in Boulder, Colorado, Bikes Belong is sponsored by the U.S. bicycle industry with the goal of putting more people on bicycles more often. There are about 400 members who are bicycle suppliers and retailers. The Bikes Belong Grants Program funds important and influential projects that leverage federal funding and build momentum for bicycling in communities across the U.S. These projects include bike paths, lanes, and routes, as well as bike parks, mountain bike trails, BMX facilities, and large-scale bicycle advocacy initiatives.

## PLAN’S INTENDED USE

The utility of this plan is in the guidance it can offer a range of potential implementers of solutions. These include Sonoma County Transit, Sonoma County Transportation and Public Works, Caltrans, Sonoma County Community Development Commission, Sonoma County Bicycle Coalition, civic/philanthropic groups, property/business owners, and non-profit entities. Guidance is also afforded to potential funders of solutions, including the Metropolitan Transportation Commission and SCTA.

MTC’s Community Based Planning Program provided a process for direct involvement of local residents in identifying challenges and opportunities for their communities. It is hoped that The Springs Community Based Transportation Plan will be fully utilized as a foundation for assisting low-income residents of the study area, so that they might improve the quality of their lives by improving their ability to safely access needed services, essential jobs, educational opportunities and enrichment activities.



APPENDIX A

# **THE SPRINGS CBTP SURVEY INSTRUMENT**



**Mailing Address: Sonoma County Transportation Authority**  
**490 Mendocino Avenue, Suite 206**  
**Santa Rosa, CA 95401**  
**Attention: Lynne March**

## Springs Community-Based Transportation Plan

### Community Survey

*We appreciate your time to fill out this survey. It will help us identify transportation gaps and potential transportation improvements for the Springs Study Area. Thank you!*

1. Do you live in the Springs Study Area (see map on back)? ☐ Yes ☐ No
2. Your age:
 

<input type="checkbox"/> 15 or under	<input type="checkbox"/> 40-49
<input type="checkbox"/> 16-19	<input type="checkbox"/> 50-59
<input type="checkbox"/> 20-29 <input type="checkbox"/> 60-69	<input type="checkbox"/> 70 and older
<input type="checkbox"/> 30-39	
3. Do you own a car? ☐ Yes ☐ No I borrow a car (\_\_\_\_\_ x week)
4. Do you drive others? ☐ Yes ☐ No
5. Are you driven by others? ☐ Yes ☐ No
6. Regarding work: ☐ I work away from home ☐ I work from home ☐ I don't work ☐ I'm retired
7. Total number of people in your household: \_\_\_\_\_
8. Your household's approximate annual income before taxes:

<input type="checkbox"/> Less than \$9,999	<input type="checkbox"/> \$30,000 – \$34,999	<input type="checkbox"/> \$70,000 – \$79,999
<input type="checkbox"/> \$10,000 – \$14,999	<input type="checkbox"/> \$35,000 – \$39,999	<input type="checkbox"/> \$80,000 – \$89,999
<input type="checkbox"/> \$15,000 – \$19,999	<input type="checkbox"/> \$40,000 – \$49,999	<input type="checkbox"/> \$90,000 – \$99,999
<input type="checkbox"/> \$20,000 – \$24,999	<input type="checkbox"/> \$50,000 – \$59,999	<input type="checkbox"/> \$100,000 – \$129,999
<input type="checkbox"/> \$25,000 – \$29,999	<input type="checkbox"/> \$60,000 – \$69,999	<input type="checkbox"/> \$130,000 or more

PLEASE GO TO PAGE 2

9. Do you experience problems getting where you want to go?

☐ Never      ☐ Sometimes      ☐ Often      ☐ Always

10. What kinds of problems do you have and where do they occur?

Walking/Biking	Driving	Bus	Other
Walking is unsafe because: <input type="checkbox"/> Sidewalks in poor condition <input type="checkbox"/> No sidewalks <input type="checkbox"/> Road crossings are unsafe <input type="checkbox"/> Other <b>Where:</b>	<b>I Don't :</b> <input type="checkbox"/> Drive <input type="checkbox"/> Have a car <input type="checkbox"/> Have a car full time <input type="checkbox"/> Have a driver's license	Traveling by bus is a problem because: <input type="checkbox"/> Bus trips take too long <input type="checkbox"/> Too much time between buses <input type="checkbox"/> Buses are late <input type="checkbox"/> Trouble getting bus info <input type="checkbox"/> Bus fare cost too much <input type="checkbox"/> No seating at bus stop	The following are too far: <input type="checkbox"/> Jobs <input type="checkbox"/> Shopping <input type="checkbox"/> Government services <input type="checkbox"/> Health services <input type="checkbox"/> Senior services <input type="checkbox"/> School <input type="checkbox"/> Childcare <input type="checkbox"/> Religious activities <input type="checkbox"/> Entertainment, social, civic activities
Bicycling is unsafe because: <input type="checkbox"/> No bike lanes <input type="checkbox"/> Other <b>Where:</b>	<input type="checkbox"/> Lack of car parking <b>Where:</b>	Bus schedules don't work; I need: <input type="checkbox"/> Earlier morning service <input type="checkbox"/> Later evening service <input type="checkbox"/> More Saturday service <input type="checkbox"/> More Sunday service	<input type="checkbox"/> Disabilities are a barrier <b>Why:</b>
<input type="checkbox"/> No bicycle parking at destinations <b>Where:</b>	<input type="checkbox"/> Cost of driving	<input type="checkbox"/> Buses don't go where I need to go <b>Where:</b>	<input type="checkbox"/> Language is a barrier <b>Why:</b>
<input type="checkbox"/> Walking or bicycling takes too long	<input type="checkbox"/> Driving feels unsafe <b>Why:</b>	<input type="checkbox"/> Taking the bus feels unsafe <b>Why:</b>	
		<input type="checkbox"/> Some bus drivers need better training <b>Why:</b>	
		<input type="checkbox"/> No bus shelters <input type="checkbox"/> No seating at bus stop <b>Where:</b>	

PLEASE GO TO PAGE 3

11. Please describe or expand on critical transportation problems and describe solutions (specify locations if possible):

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12. What three improvements would make it easier for you to travel?

1. 

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2. 

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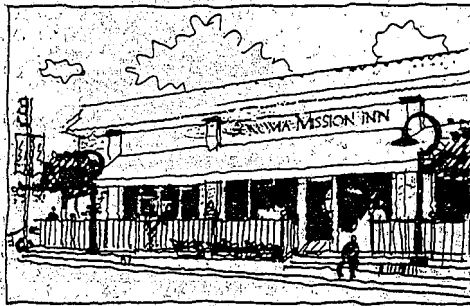
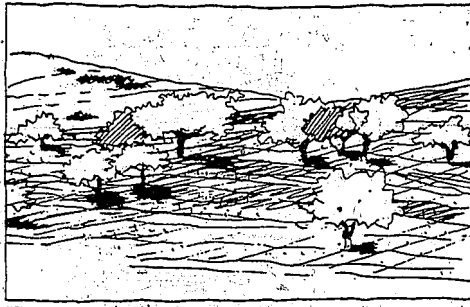
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3. 

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**THANK YOU FOR YOUR ASSISTANCE . WE TRULY APPRECIATE YOUR INPUT!**



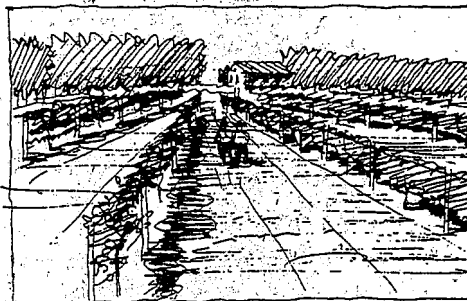


sonoma county  
community redevelopment agency

# HIGHWAY 12

## design guidelines

november 1994



## **ACKNOWLEDGMENTS**

### **County Staff**

Ken Milam, Director, Permit and Resource Management  
Ed Walker, Director, Department of Transportation and Public Works  
Nick Chase, Sonoma County Planning Department  
Bill O'Connell, Department of Transportation and Public Works

### **Sonoma County Community Redevelopment Agency**

Michael J. Cale, Chairman  
James Harberson, Second District  
Tim Smith, Third District  
Nick Esposti, Fourth District  
Ernie Carpenter, Fifth District  
Janie V. Walsh, Executive Director  
Tom Bane, Redevelopment Manager

### **V.I.P. Citizens Committee**

Hal Beck, Executive Director, Sonoma Valley Chamber of Commerce  
Sandi Seamas, Chamber Director  
Fred Gerdes, Sonoma Valley Realty  
Doug McKesson, Goodtime Bicycle  
Reiner Keller, Architect  
Bill Blum, Sonoma Mission Inn & Spa  
Alan Medina, Parsons Hardware

---

### **Sonoma Valley Citizens Advisory Commission**

Robert J. Gemmell, Co-Chairperson  
Pamela Stephens, Co-Chairperson  
Alden Brosseau  
Jerry Bernhaut  
Steve Wolf  
Peggy Bair  
Jerold Tierney  
Y. Tito Sasaki  
Cathy Dougherty  
Charlie Cooke  
Lauren Keyson  
Niels Arthur Chew  
Catherine Wade Shepard  
Ignacio Vella

### **Consultants**

Gates & Associates, Danville, California

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## ***1. CORRIDOR OVERVIEW***



# ***INTRODUCTION***

Chapter I

INTRODUCTION

introduction

*introduction*

# 1. Introduction

---

Over the past 100 years, the Sonoma Valley has evolved from a landscape of vineyards and country homes to a series of communities. Although the vineyards still remain, the continuing growth pressures have brought increased traffic and suburbanization of the Valley.

The project area is comprised of small communities, many of which were developed around the various hot mineral springs resorts including Feters Hot Springs, Agua Caliente, Boyes Hot Springs and El Verano. These communities are linked by Hwy 12. Through time, the boundaries between these communities have blurred to create a unified area we have identified as "The Springs Area". Unlike the rapid development of the City of Sonoma to the South, and Santa Rosa to the North, the Springs Area has experienced a more erratic evolution resulting in a variety of land uses. The mixture of older summer homes converted to offices, new residential developments, commercial strip retail, auto service centers and Main Street shops create use conflicts, but also contribute to the eclectic charm of the community. However, with continuing growth and traffic pressures, the historic country ambiance could be sacrificed for commercial centers and signage, and a widened roadway with Caltrans light standards and curbs. It is the desire of the community to retain the rural ambience of the Hwy. 12 Corridor through the Springs Area by limiting the road width to 3 travel lanes. The County General Plan should be amended to reflect this direction.

## About this document:

The purpose of the Hwy. 12 Design Guidelines is to provide a vision and a design vocabulary that will lead to the beautification of the Corridor, through both public and private efforts. The vocabulary aims to be flexible, nurturing eclectic expressions without stifling creativity. The guidelines are intended as a supplement to the existing Sonoma County-wide ordinances, standards and guidelines.

This document has been organized into three parts; Corridor Overview, Design Guidelines and Site Elements Appendix.

**The Corridor Overview** includes information on the design goals for the Springs Area, an analysis of the existing setting and an overall design concept for the enhancement of the Corridor. Included in the design concept are sketch plans for key Study Areas which illustrate potential public and private improvement collaborations.

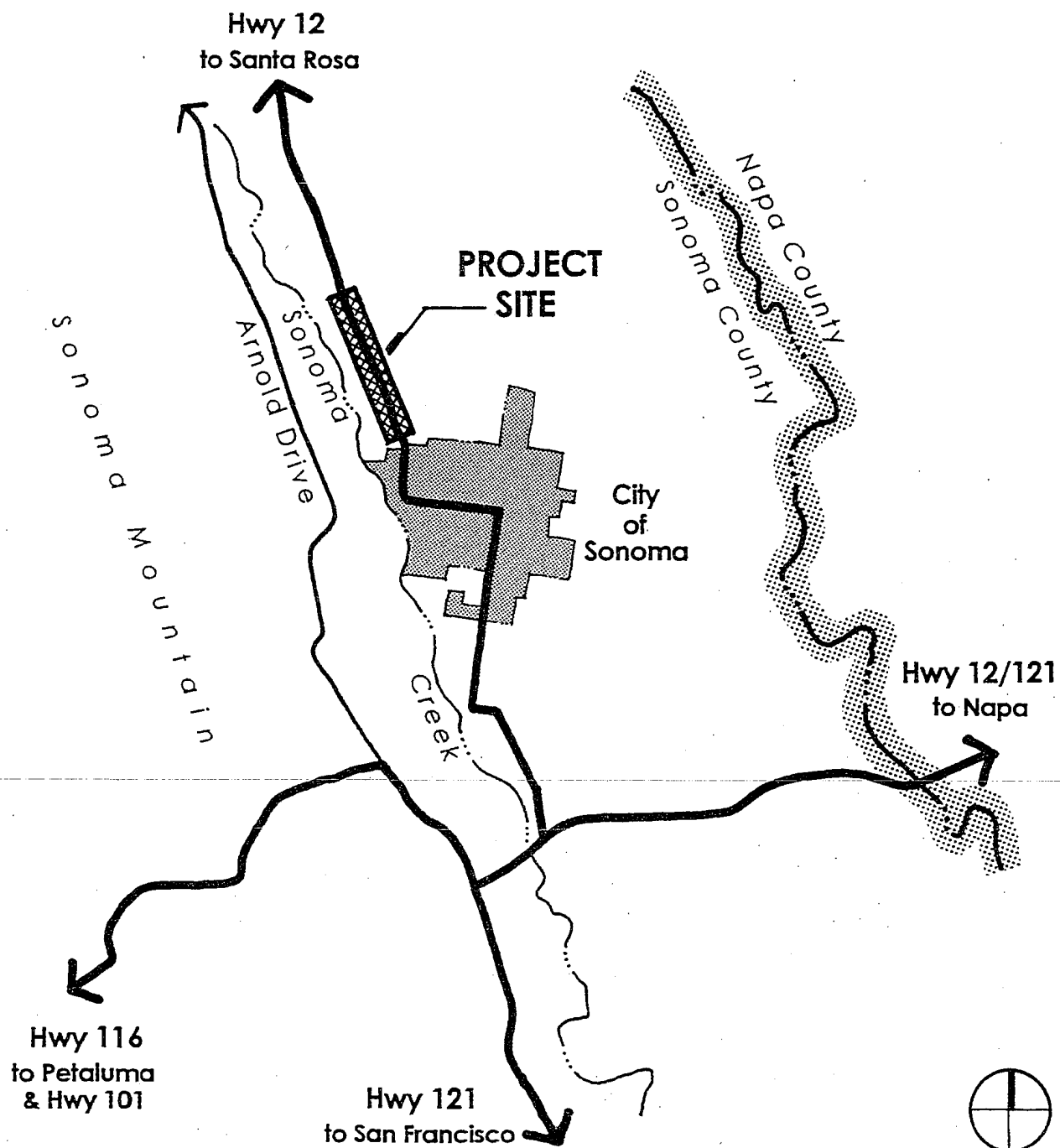
**The Design Guidelines** include design criteria for private development to ensure the consistency of each individual project with the overall character of the Corridor.

**The Site Elements Appendix** is a palette of street furniture, fencing, lighting and landscape materials which have been selected for their appropriateness to the country image of the Corridor in the Springs Area.

These guidelines, are just that, guidelines. Each private development proposal will need to be evaluated on its own merits. The Study Areas and recommended public improvements such as plazas, traffic signals, walkways, and landscaping, will need to be precisely developed and subjected to public review and comment at all stages of the review process. The process of developing this study included:

1. Analysis of the existing potentials of the unique setting and essence of the community identity.
2. Working with representatives of the community to create goals and objectives for the Springs Area.
3. Developing an overall concept for the area by defining the entries, nodes and center which provide the community structure.
4. Developing standards for a unified streetscape, which could be incrementally implemented.
5. Defining a design criteria for use by public agencies for the evaluation of future development and rehabilitation to ensure consistency with the Springs Area.
6. Selecting a palette of appropriate materials and colors, architectural images and landscape elements to guide parcel owners in the improvement of their properties.

The vision described in this study will not be realized instantaneously. Rather, the improvements will be implemented by the separate and joint efforts of public agencies, private developments, and volunteer organizations. It is the desire of the Sonoma Valley Citizens Advisory Commission that the Redevelopment Agency implement pilot public improvement projects to demonstrate built examples, and to keep the momentum for beautification strong. The guidelines are intended to create a pattern in which improvements can incrementally occur, yet allow the flexibility for the individual expressions which have made the Springs Area visually distinct.



REGIONAL LOCATION MAP

## 1.1 THE PLACE

The Springs Area is a unique place. It is the intent of these guidelines to preserve and enhance the components that contribute to the special qualities of the area, which include:

### 1. Natural Hot Mineral Springs

Historically, the area has become famous as a hot springs resort area. The Sonoma Mission Inn & Spa continues to be an international destination for "taking of the waters."

### 2. Dominance of a Rural Atmosphere

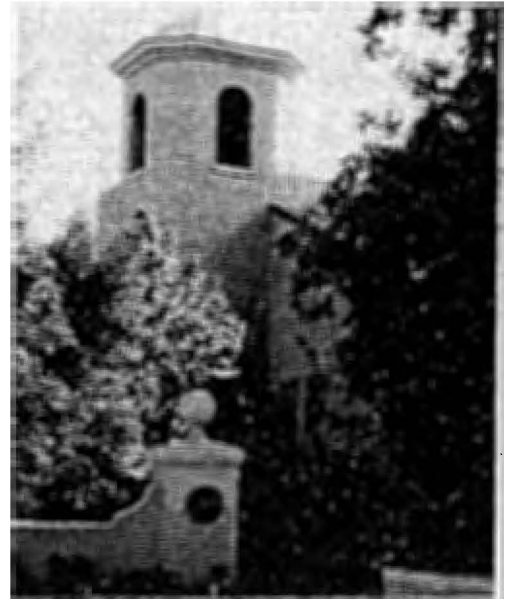
The rolling topography of the meandering roadway, tree-lined creeks and swales, mature oaks and distant views of golden hills are strong components of the visual setting.

### 3. Agrarian Heritage

The historic imprint of civilization's taming of the land is reflected in the open air Farmers Market, dry stack walls, split rail fences, landscapes of orchards and vineyards which enclose the community.

### 4. Architectural landmarks.

The charming mixture of eclectic farmhouse, Mediterranean and historic architecture is a key component of the Corridor's ambience.



*Mediterranean architecture landmark.*



*Agrarian landscapes enclose the community.*

## 1.2 PLANNING BACKGROUND

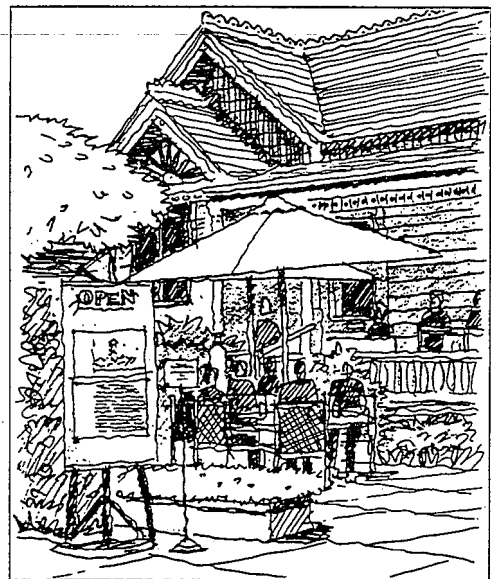
In 1978, the South Sonoma Valley Area One Specific Plan was prepared by the Sonoma County Planning Department. It focused on the four communities that make up the Springs Area. Specific goals of the Area One Study included the enhancement of the rural small town atmosphere and encouragement of tourist-related facilities and activities along the Hwy. 12 Corridor. The Area One Study proposed special land use designations to avoid strip-type development along the Corridor. The guidelines in the Study suggests overall beautification of the Corridor and provisions for sidewalks, landscape setbacks and street trees. This study drew attention to both the role of Hwy. 12 as a regional corridor and its importance in the Springs Community image.

The Springs Area is under the jurisdiction of the County of Sonoma, and the Sonoma County Community Redevelopment Agency has jurisdiction over redevelopment activities in the Sonoma Valley Redevelopment Project Area. In November 1984, the Board of Supervisors and the Community Development Commission adopted the Sonoma Valley Redevelopment Plan. One of the Plan's goals is "to create for the Sonoma Highway commercial district an efficient, attractive and identifiable commercial neighborhood in which existing and new commercial establishments and professional offices can capture a share of the growing market demand for goods and services in the area."

A related objective of the Plan is to "assist current property owners and businesses, and new developers to intensify commercial and related activities in the Sonoma Highway commercial district through rehabilitation of structures and new construction." The Plan states that to accomplish this objective, the Redevelopment Agency may "encourage a stronger community identity for the Sonoma Highway commercial district and the general beautification of this area through the adoption and implementation of design standards which will establish, among other requirements, a uniform sign theme and landscaping standards."

Under this charge the Redevelopment Agency, late in 1993, retained the firm of David Gates & Associates, urban designers and landscape architects, to prepare the design guidelines for the Hwy. 12 corridor. The guide-

lines will benefit the Project Area by improving the appearance of the development along Hwy. 12, and will benefit developers and property owners by informing them about design requirements early in the developmental process. The Consultant Team was directed to work with the community to develop guidelines for the corridor which consists of three lanes (2 travel lanes and a continuous left-turn lane).



*Attractive, identifiable commercial establishments.*

### 1.3 PUBLIC PARTICIPATION

The public has played a vital role in the development of these guidelines. A steering committee, consisting of key members of the community, as well as representatives of the County Planning Department, the Department of Transportation and Public Works and the Redevelopment Agency, met with the Consultant Team throughout the planning process to discuss background information and identity issues, evaluate alternatives and provide insights into Community goals. The Consultant Team, with the assistance of the Steering Committee, held three public workshops, which were publicized by flyers, articles and newspaper notices.

On February 10, 1994, citizens who were interested in improvements along the Hwy. 12 corridor attended the first workshop. The Consultant Team summarized background information on existing conditions and facilitated a discussion on the image of the corridor. Suggested goals include the creation of a distinct image for the Springs Area, recognition of its Hispanic heritage, preservation of the rural atmosphere, development of a central plaza or town square, provision for bike circulation and a safe, friendly pedestrian environment, and encouragement of the growth of "local" businesses. Major concerns included the impact from widening the corridor to accommodate five travel lanes as indicated by the General Plan, problems arising from the development of new businesses under current traffic generating guidelines, and the overall impact on existing uses as designs are implemented.

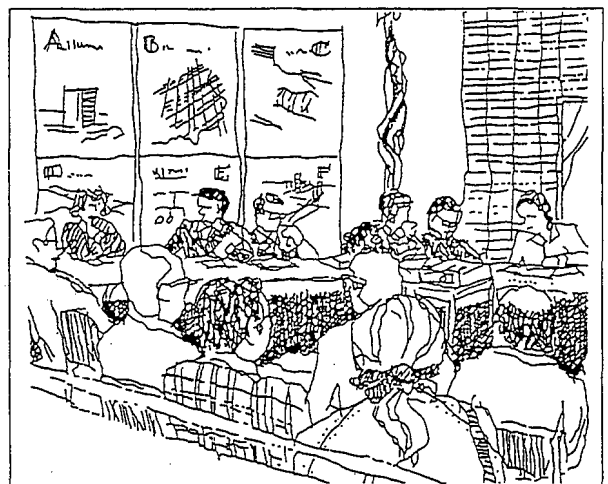
On April 25, 1994, a Community Workshop was held at Flowery Elementary School to discuss design criteria on setbacks, walkways, parking, landscape characteristics, signage and special features. This included the presentation of design concepts for the El Verano Gateway, Thomson Node, Mountain Avenue Node and Boyes Hot Springs town square, as well as prototypical treatment of residential, commercial and downtown or "Main Street" uses along the corridor.

The design concepts addressed major concerns and included the provision of a comprehensive regional bike plan, the cost of improvements, and implementation time frame. Although a comprehensive bike plan is not a part of this study, the guidelines encourage the development of bike amenities. The guideline recommendations are scaled to be cost appropriate to the types of development along the corridor.

In addition to the Community Workshop, a presentation was made to the Sonoma Valley Citizen's Advisory Commission to solicit input on regional issues. At the August 24, 1994 meeting, the Advisory Commission commended the Sonoma Valley Chamber of Commerce for their efforts in bringing the design guidelines to fruition and giving their endorsement to the study.

The community has been actively involved in the development of these guidelines, and they are strongly encouraged to continue their efforts in the beautification of the corridor. Their involvement may take place in a variety of ways:

1. Participation in beautification programs like volunteer cleanup days and streetside planting events.
2. Development of a Chamber of Commerce sponsored awards program in recognition of design efforts like most creative new sign, most attractive landscape edge and best new residential entry.
3. Participation in future updates of the design guidelines.
4. Input of design ideas into public improvement projects including the Southern Gateway and Boyes Hot Springs Segments, and undergrounding of electrical lines.



*Information gathering through meetings with the public.*



# ***THE SETTING***

Chapter 2

THE SETTING

the setting

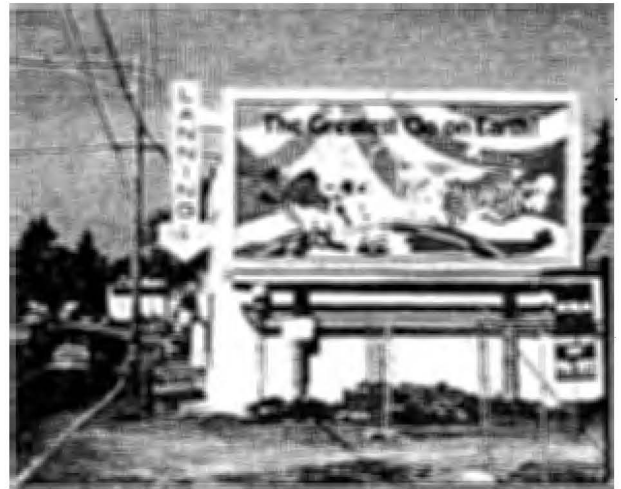
*the setting*

## 2. The Setting

The Springs Area is situated in the southern Sonoma Valley between the Mayacmas Mountains and Sonoma Mountain. The Valley is known as a prime wine-producing region and is visited yearly by hundreds of thousands of tourists. Regional access to the area is primarily provided by Hwy. 12. The Hwy. 12 corridor winds through the valley linking the City of Sonoma and Santa Rosa. This three-lane State highway bisects the unincorporated Springs communities of Agua Caliente, Boyes Hot Springs, El Verano and Fetters Hot Springs. The following are features of the project area.

1. The Springs Area has evolved without the benefit of a predictable planning process. This pattern has contributed to both the charm and the blight in the area.
2. Large billboards, overhead utilities, unmaintained vacant lots and dilapidated structures detract from the corridor's general appearance.
3. The vacant lands located adjacent to the Hwy. 12 corridor allow opportunities to provide commercial and professional services to strengthen the local economy and to provide centralized parking facilities.
4. The abundance of mature trees, dry stack walls, rail fences and rolling topography create a pleasant rural ambience. The lack of sidewalks and bicycle lanes discourages non-vehicular circulation.
5. The Sonoma Mission Inn and other historic buildings provide an architectural theme to improve the visual quality of the built environment.
6. Numerous creek and swale crossings along the corridor provide opportunities for the natural vegetation to enclose the roadway and to highlight the bridge character.
7. Homes located adjacent to stores contribute to potential conflicts, but also create a rich, small town environment.

8. Numerous examples of individual artistic expressions are evident along the corridor through merchant's signage, landscape display, or architectural character. This personal quality is integral to the character of the Springs Area. Design guidelines must provide the flexibility to nurture artistic expressions.



*Billboards and overhead utilities detract from the setting.*



*Flexible guidelines nurture creative displays.*

## 2.1 PEDESTRIAN CIRCULATION

The non-cohesive sidewalk system, established mainly by individual businesses and owners, discourages pedestrian activities that could otherwise help to alleviate the street parking congestion. Although significant emphasis has been placed by the community on the provision of a safe and attractive pedestrian circulation, the sidewalk system along the corridor is still very inadequate. Frequently, pedestrians have to walk along the road edge on the dirt paths created through continual use. Despite the lack of a sidewalk system, a substantial level of pedestrian activity still occurs along the corridor.

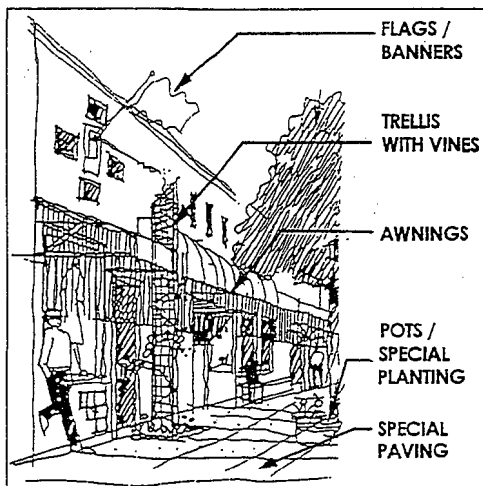
### Issues:

1. The existing situation consists of discontinuous, uneven sidewalks, staggered locations, and unpaved dirt paths, which do not facilitate pedestrian circulation. The short stretches are also of varied elevations with random steps that do not meet handicapped accessibility requirements.
2. Walkways are frequently disrupted by driveways or front setback parking.

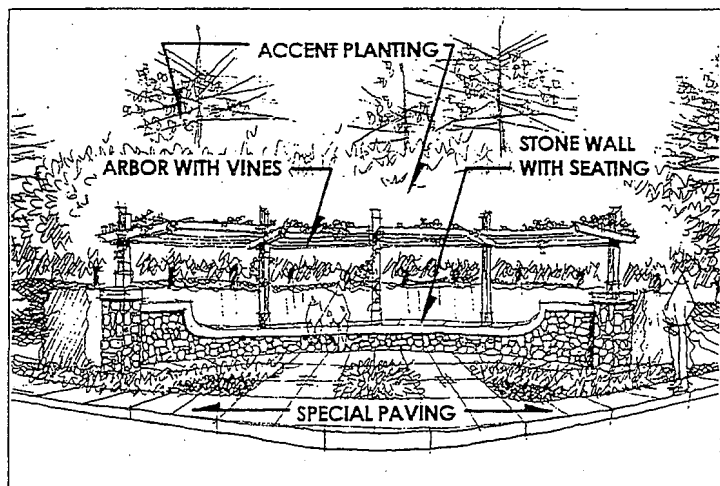
3. Additional safe pedestrian crossings are needed at street junctions.
4. The area lacks pedestrian amenities such as rest areas and shade trees. There are no thematic streetscapes uniting various areas and nodes. Sidewalk widths, locations and materials vary. There is no strong existing sense of pedestrian priority, nor soft landscape elements that provide climatic protection.
5. New sidewalks adjacent to the curb and parked vehicles are not compatible with the country character.

### Recommendations:

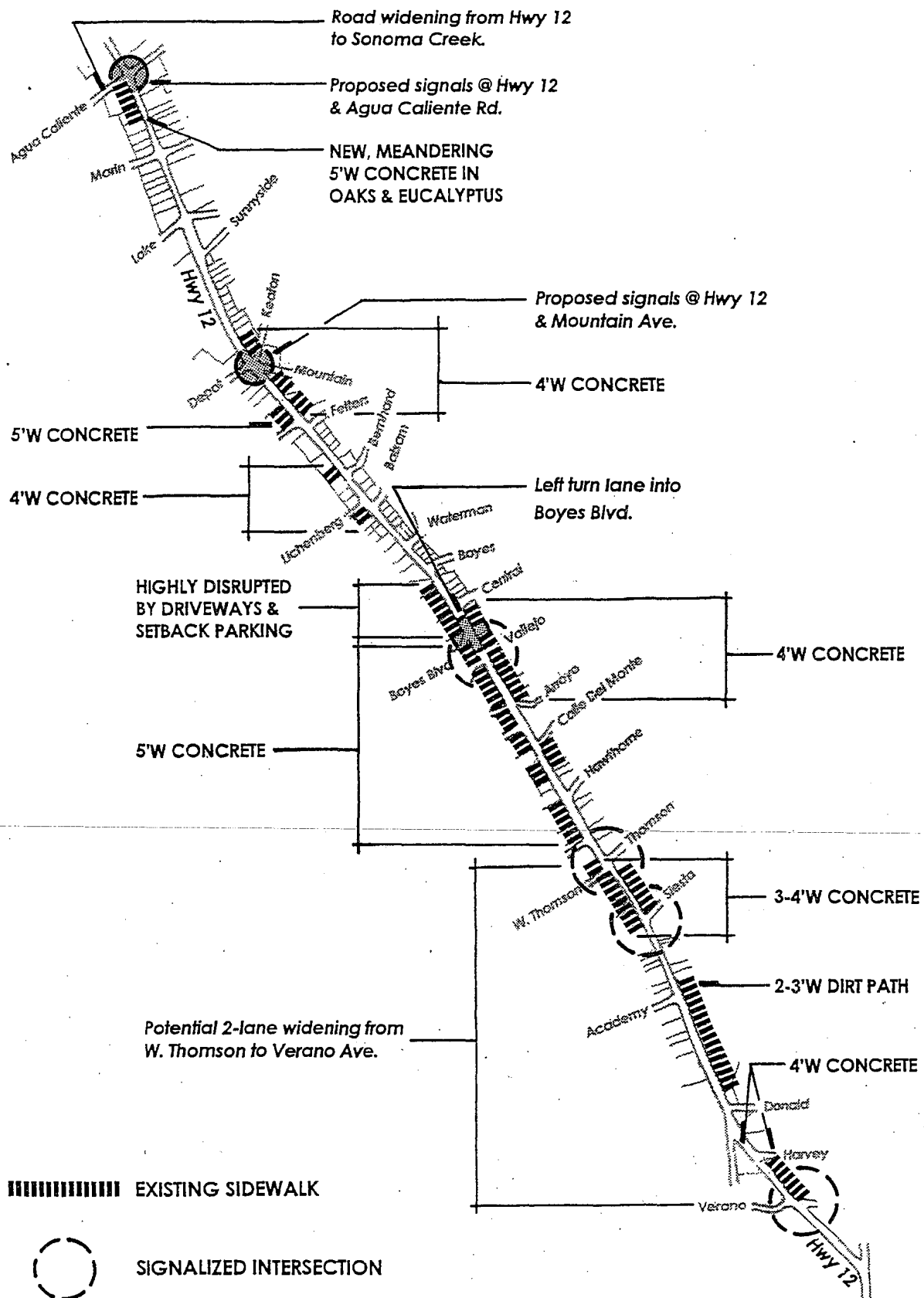
1. Create themes and use different sidewalk treatments to identify different character areas, (commercial, residential, etc.).
2. Provide focus at strategic locations with activity hubs for a higher quality streetscape and pedestrian circulation.
3. Enhance pedestrian use by providing a continuous circulation system with pedestrian amenities.



*Pedestrian-scaled sidewalk.*



*Focus at strategic locations incorporating pedestrian amenities.*



## PEDESTRIAN CIRCULATION

## 2.2 PARKING PATTERNS

The provision of adequate parking within reasonable proximity to stores is a critical part of the success of any retail environment. Frequently, downtown areas are perceived to have a parking shortage, when in fact reconfiguration of the parking areas, increased awareness of off-street parking, pleasant convenient links between parking areas and store entrances, and separation of employee and customer parking can alleviate parking problems.

Hwy. 12 has not been planned to accommodate street parking. Existing unplanned parking 'littering' the linear site has resulted in a dishevelled appearance. Ultimately, parking requirements for businesses must be met through off-street parking, either in public or private parking lots. Many of the businesses along Hwy. 12 are presently providing off-street parking. Careful planning could result in ample lots that are strategically located with emphasis on pedestrian circulation.

### Issues:

1. Areas of uncontrolled street parking on dirt easements between the edge of Hwy. 12 and private properties present a negative image. Numerous parking areas require vehicles to back onto Hwy. 12.
2. Random parking on sides, front and rear of properties with spillover onto adjacent vacant lots does not encourage the use of these parking areas, nor does this enhance the appearance of businesses.
3. Multiple driveways and street parking disrupt pedestrian circulation. Unlandscaped parking areas present a disorganized image to users of the corridor.

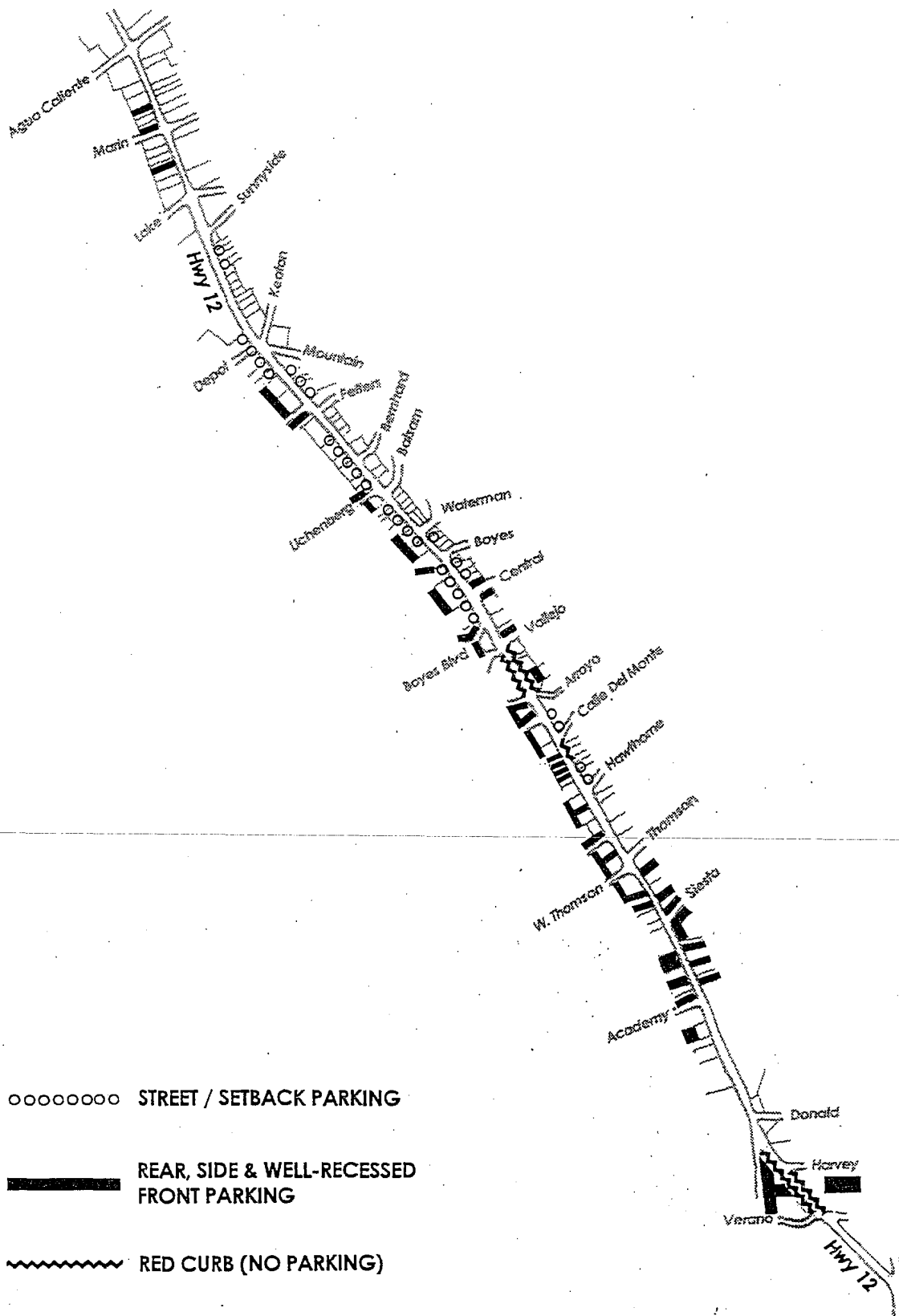
### Recommendations:

1. Through parcel consolidation and cross easement, off-street public parking areas could be appropriately developed along the project corridor. This will ultimately ease parking congestion along the Hwy. 12 edge, allowing for streetscape enhancement to take place. The Redevelopment Agency can provide development assistance for parcel consolidation.

2. Provide attractive, landscaped pedestrian routes between the parking area and the facilities within its area of service.
3. Shared parking and driveways (preferably at rear of buildings), will enhance the quality of pedestrian circulation along the building frontages.



*Existing front setback parking*



## PARKING PATTERNS



## 2.3 EXISTING LAND USE PATTERNS

Analyzing the current land use allows for future reinforcement of the existing positive features. The existing land use pattern classifies retail uses according to their user orientation. In the **auto-oriented** retail, the building is oriented towards the parking area, and has little relationship to the adjacent use. The **pedestrian-oriented** retail building is accessed from the sidewalk, with the parking area being hidden. This type of building will typically relate with adjacent uses to create a mutually supportive shopping environment. Thus, the pedestrian-oriented uses tend to cluster around the important nodes along the corridor.

The multi-family residences along the corridor tend to be internally-oriented towards a common open area, buffered from Hwy. 12. The single family residential areas are typically made up of older homes established prior to the commercialization of the area. These homes face Hwy. 12 and are strongly impacted by this roadway. Residential areas which are in close proximity to retail uses will benefit from enhanced pedestrian circulation.

### Issues:

1. There are undefined boundaries between the various land uses. Functions are randomly intermixed.
2. Unkempt vacant lots and buildings detract from the properties which are well-maintained.
3. The corridor is monotonous, lacking focus, rhythm, and high points in its linear stretch.
4. Mixture of land uses frequently creates conflicts and does not provide the supportive vitality of a retail center.

### Recommendations:

1. Vacant parcels can be developed along with adjacent businesses to provide integrated parking. This will contribute to the beautification and economic revitalization of the area.

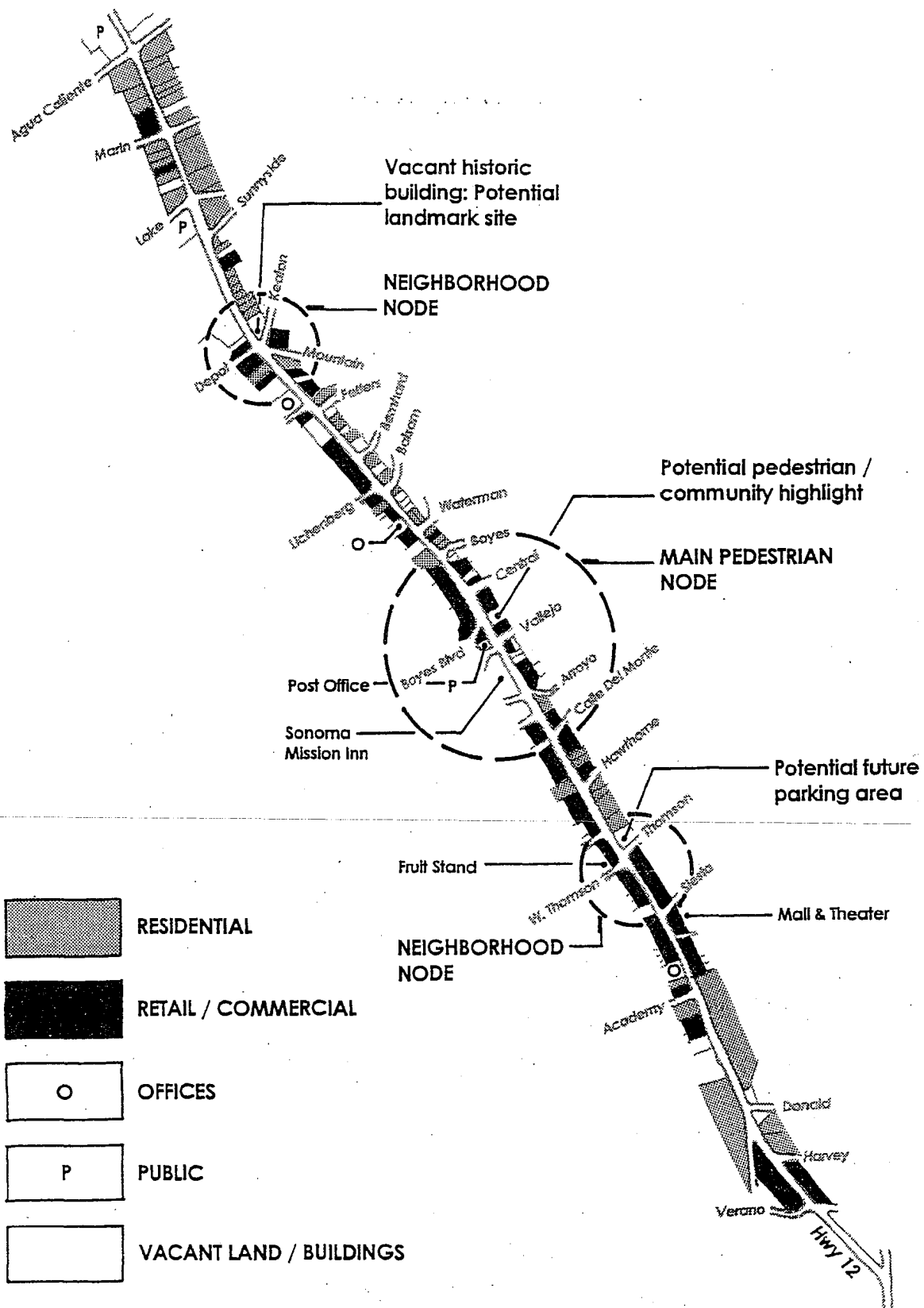
2. Reinforce the positive existing use patterns. Concentrate similar uses to create destinations and a mutually supportive business environment.
3. Allow for the conversion of non-conforming residential use to commercial use such as boutique, craft shop, chiropractor office, etc.



*Pedestrian-oriented cafe accessible from the sidewalk.*



*Single family residences along the corridor.*



## EXISTING LAND USE PATTERNS

# ***CORRIDOR DESIGN CONCEPT***

Chapter 3

CORRIDOR DESIGN CONCEPT

corridor design concept

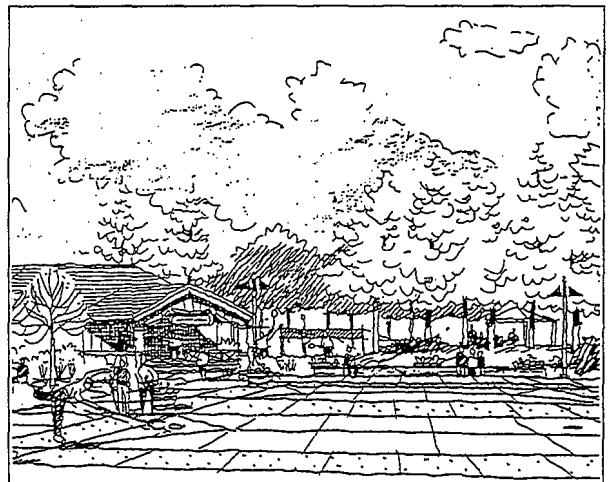
*corridor design concept*

### 3. Corridor Design Concept

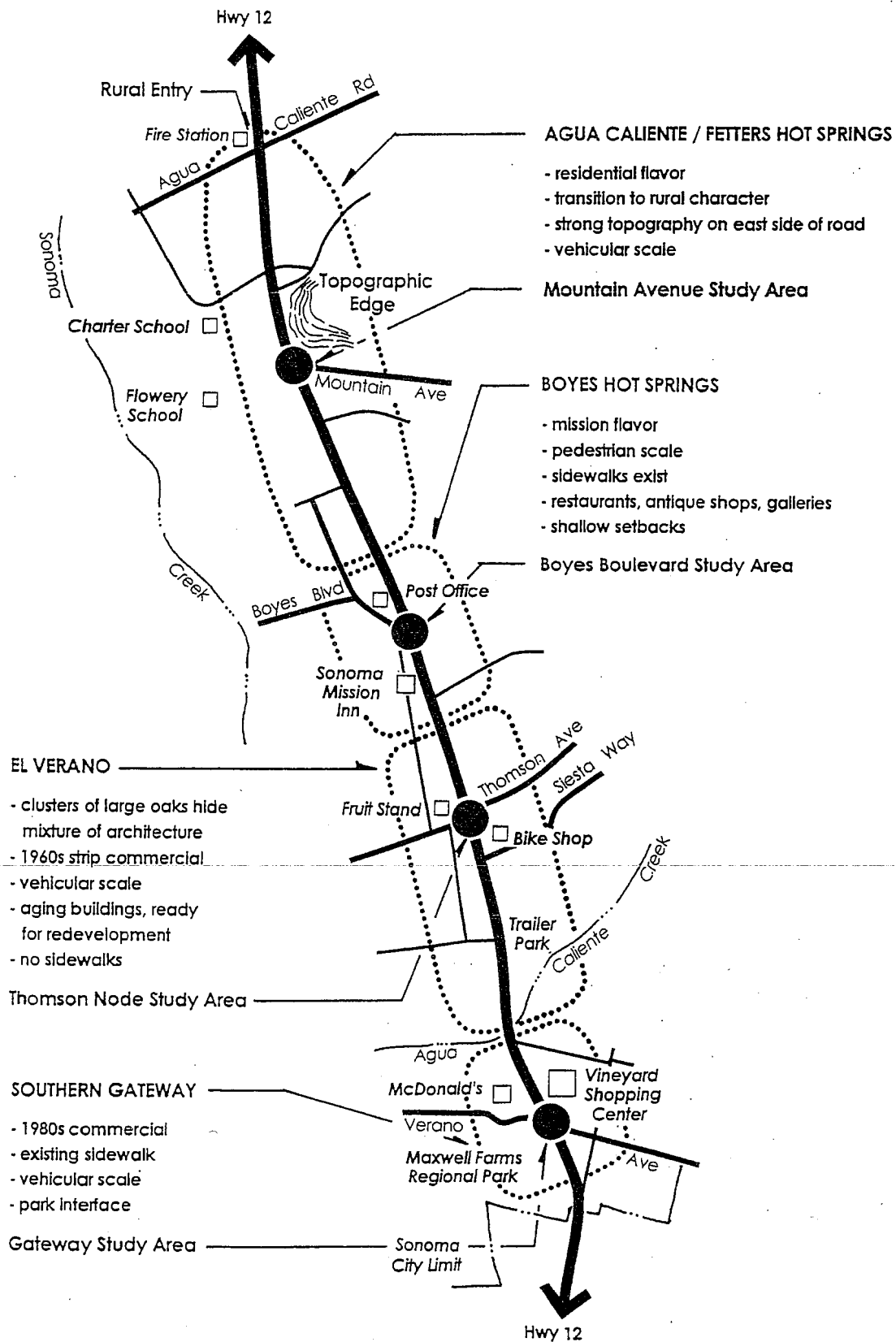
#### THE VISION

In developing a vision for the Springs Area, a number of overall goals for the beautification of the corridor emerged.

1. Create an **identity for the corridor** as it passes through the Springs Area which is distinct from the City of Sonoma and other areas through the use of a common streetscape character and the creation of clearly defined entry points. Enhance the entries at El Verano and Agua Caliente to create defined gateways to the Springs Area.
2. Emphasize the **natural and historic resources** of the Springs Setting to create a unique image for the area. Creeks & swales should be recognized as special amenities. Where the roadway crosses swales and creeks, use special railings with historic character to create a 'bridge' feeling. Along the streetscape, utilize a rural palette of materials such as picket fences, rock walls, Mediterranean and craftsman style architecture.
3. Encourage **local servicing retail uses**. Create incentives such as parking bonuses for the development of stores and businesses which meet the daily needs of local residents, thus reducing auto trips required to meet these needs outside of the area. Cluster similar uses together to create a mutually supportive shopping environment. Establish pedestrian links between residential areas and shopping opportunities.
4. Preserve the **country character** of the corridor. Restrict the road width to three travel lanes with 8 ft. shoulders. The ultimate level of traffic on Hwy. 12 will be compromised by restricting the road width to 3 lanes. Explore options for fluting the roadway at major intersections to provide additional turn lanes. Provide a generous landscaped setback with meandering walks and large shade trees. Avoid an urbanized appearance of standard curb and gutter with overlit roadways.
5. Create a **pedestrian and bicycle friendly atmosphere**. Provide pleasant continuous walkways along the corridor with shade trees and seating areas which enhance pedestrian use. Provide bridges at creek crossings with widths that can safely accommodate pedestrian and bicycle traffic. Explore opportunities to provide links to the regional bike system and destinations along the corridor.
6. Establish a **central community-wide focus** for the Springs Area. Develop a town square in the Boyes Springs area by creating a plaza and park area which encourages casual gathering, provides regional information for tourists, accommodates festival and outdoor displays, or functions as a staging area for bike races or wine making demonstrations.
7. Explore options for the **maintenance** of the streetscape to ensure a consistent quality. Options might include the formation of a Lighting and Landscaping Assessment District, or maintenance by individual property owners.
8. Recognize the uniqueness of each segment along the corridor. Create an **identifiable focus** or node within each segment that highlights the special character of the setting. The Hwy. 12 Corridor is comprised of four visually distinct segments:
  - Southern Gateway Segment
  - El Verano Segment
  - Boyes Hot Springs Segment
  - Agua Caliente / Feters Hot Springs Segment



Town square as a community-wide focus, used for gathering.



## CORRIDOR CHARACTER



*Vineyard Shopping Center with unscreened elevated parking.*



*Boyes Blvd: potential for town center and pedestrian plaza.*



*Eclectic charm of existing residences.*



*Existing stone monolith at the Mountain Ave study area.*



*Mediterranean flavor of Sonoma Mission Inn.*



*Rural northern entry defined by oaks and eucalyptus.*

## CORRIDOR IMAGES

### 3.1 SOUTHERN GATEWAY SEGMENT

The Southern Gateway marks the boundary between the Springs Area and the City of Sonoma. The existing McDonald's and Vineyards Shopping Center create a modern suburban commercial character not found along the rest of the corridor. The roadway has been widened to accommodate additional traffic lanes. Standard curb, gutter and sidewalk have been constructed along the road. McDonald's and the Vineyard Shopping Center dominate the intersection image with the ubiquitous "golden arches," blue roofs and unscreened views of parked cars elevated above the sidewalk.

The presence of Maxwell Farms Regional Park, as well as significant areas of vacant land at the intersection due to roadway realignment, provide the area necessary to create a gateway statement. As Hwy. 12 drops down to Agua Caliente Creek, the road bends and focuses on another remnant right-of-way triangle where a landscape feature could occur. The creek provides an appropriate termination to the gateway statement.

Both the County and City of Sonoma should work in close cooperation to ensure harmony between these recommendations and the existing, as well as future, design guidelines of the City of Sonoma.

#### Gateway Study Area

##### **Issues:**

1. Verano Avenue, which is a major intersecting street and a vital identity component of the Springs Area, functions as the gateway to this project corridor. Currently, the visual image of the area is dominated by franchise fast food restaurants and the Vineyard Shopping Center, both of which do not reflect the special attributes of the Springs setting. A gateway statement should be established at the intersection of Hwy. 12 and Verano Avenue.
2. Being a major pedestrian and bicyclist destination, Maxwell Park should be given careful consideration in respect to its transition and linkage with the project corridor to the north.
3. On the eastern edge, there is an existing sidewalk bordering the shopping center.

##### **Goal:**

1. Create a memorable gateway to the Springs Area which is unique to the City of Sonoma.

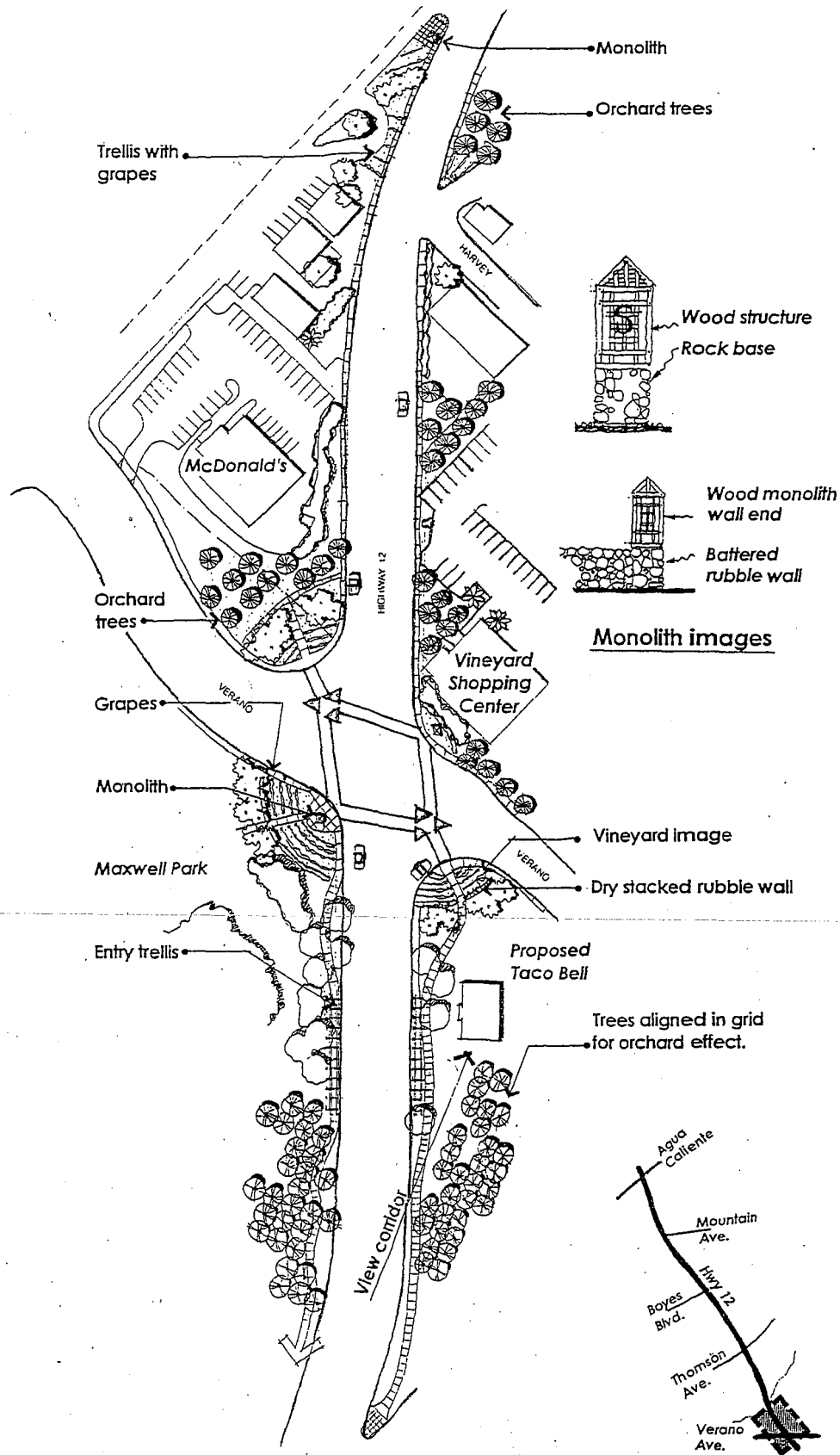
##### **Recommendations:**

1. Utilize orchard grid plantings of ornamental trees and vineyard remnants to create a distinct streetscape image which reflects agrarian roots of the area. Orchard grids can extend into park edges, and whenever possible, private development areas.
2. Establish a trellis system to border the road and announce the gateway area.
3. Develop corner treatments to highlight the intersection by using low stone walls, special plantings, and thematic monoliths enclosed by tree masses. Use grape vineyards in concentric arcs to define the intersection.
4. Extend orchard and vineyard streetscape northwards along the corridor, terminating in a focal element (monolith, specimen tree) at the prominent bend in the road.
5. Develop further detailed design studies by retaining a consultant or by holding a design competition.



*Gateway view of orchard grids and vineyards.*





## GATEWAY STUDY AREA

### 3.2 EL VERANO SEGMENT

The El Verano segment of the corridor is characterized by a mixture of older commercial developments set back from the road, a mobile home park, a gas station, a number of restaurants, a bike shop, a small shopping center, and parking areas. The combination of uses is partially obscured by matured oaks which line the roadway.

The open air fruit market at Thomson Avenue is a significant landmark for the area. The recent rehabilitation of the Fiesta Shopping Center and the introduction of a theatre into this node further enhance the pedestrian vitality of the area. However, the offset alignments of Thomson Avenue create traffic conflicts.

#### Thomson Node Study Area

##### **Issues:**

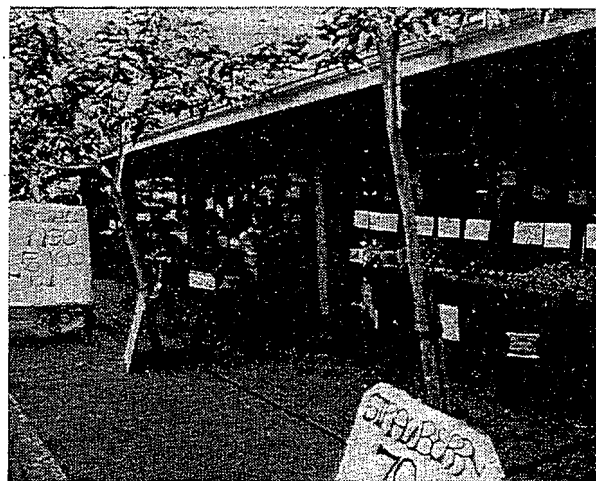
1. The Fruit Market with its open air stands that spillover onto the sidewalk is a visual landmark as well as a major destination along the Hwy. 12 Corridor. The bike shop has an architectural character similar to the fruit market. There are numerous mature trees that help to soften the appearance of the corridor. However, this area lacks visual continuity.
2. Although the majority of the area does not have existing sidewalks, there is still a significant level of pedestrian traffic along the edge of Hwy. 12. With the development of the public parking lot on East Thomson Avenue, pedestrian use will increase. The presence of the bike shop contributes to the demand for adequate bicycle facilities in the area. The offset alignment of East and West Thomson result in circulation conflicts.

##### **Goals:**

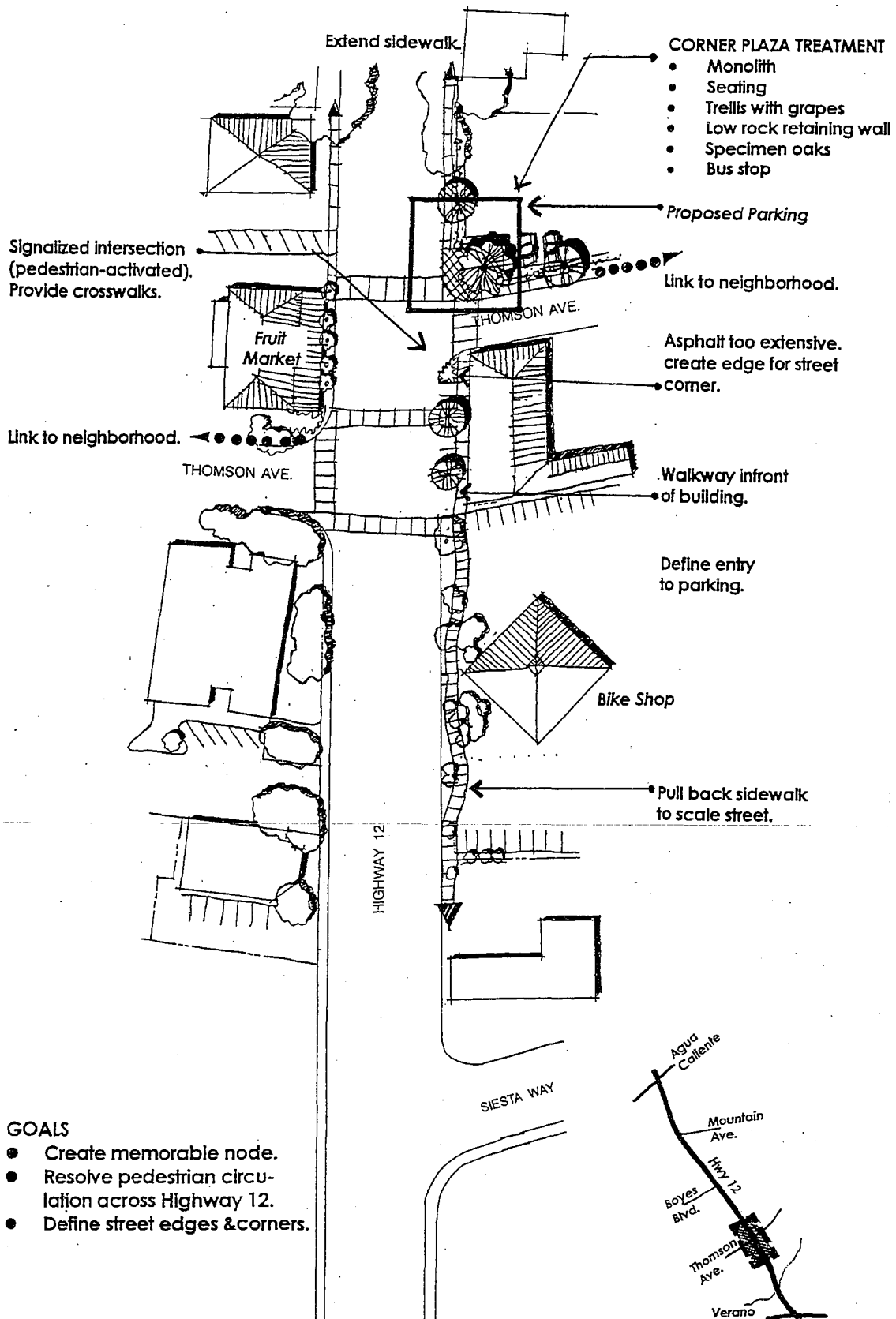
1. Create a pedestrian-oriented focus for the El Verano community.
2. Enhance the image of this area.
3. Resolve circulation conflicts.

##### **Recommendations:**

1. Consolidate East and West Thomson Avenue into a single extended intersection. Provide pedestrian signals and crosswalks.
2. Provide a continuous sidewalk along the corridor, linking with residential areas. Locate sidewalk away from the curb whenever possible, providing a planting area for street trees.
3. Use sidewalks and curbs to define the street edge, and landscaping to minimize expanses of parking especially at street corners.
4. Explore opportunities to create a small corner plaza near the public parking area. Amenities could include benches, specimen oaks, a thematic monolith identifying the El Verano Area, low terraced walls and flowering plants.
5. Develop a thematic sign which is used to direct motorists to off-site public parking throughout the corridor.
6. Screen views of parking areas with low walls, shrubs, rail fences or arbors with vines.
7. Clearly define and consolidate driveway access to off-site parking areas.
8. Encourage relocation or reconfiguration of the parking areas which spillover onto Hwy. 12. Explore opportunities to provide parallel parking with pedestrian walk along store fronts, or off-site parking to eliminate this problem.



*The fruit market is a visual landmark.*



#### GOALS

- Create memorable node.
- Resolve pedestrian circulation across Highway 12.
- Define street edges & corners.

### THOMSON NODE STUDY AREA

### 3.3 BOYES HOT SPRINGS SEGMENT

The Boyes Hot Springs segment is focused around the Sonoma Mission Inn. The Post Office has a distinct character from the adjacent segments. Here the country road character changes to a small town "Main Street". The building setbacks are shallow, creating an architectural edge along the street. Parking areas are on side streets or hidden behind buildings. The architecture, width of sidewalk, formal street tree layout, and types of uses create a pedestrian friendly atmosphere. The Mediterranean architecture and pastel colors of the Inn dominate the architectural character of this node.

#### **Boyes Boulevard Study Area**

##### **Issues:**

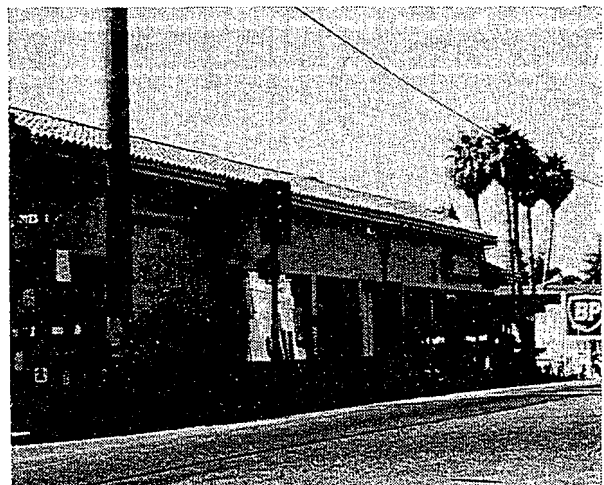
1. The heart of the Springs Area is defined by the Sonoma Mission Inn, the Post Office and other retail storefront areas which form the town center. Unlike other parts of the Springs Area, this node is consistently defined by an architectural edge. The Inn, a major regional destination, sets an elegant theme of light pastel Mediterranean architecture, generous landscaping and a celebration of the historic hot spring character of the area. Boyes Boulevard is a major intersecting street. The sidewalk in front of the Post Office is narrow, and pedestrian circulation is further constrained by obstacles such as mail boxes and telephone poles.
2. The full width of the street is utilized for traffic lanes. Public parking is provided in a narrow band behind and along the street to the north of the Post Office building. The storefront edge is disrupted by vacant lots and parking areas directly abutting Hwy. 12. Further development may exacerbate the parking problem.

##### **Goals:**

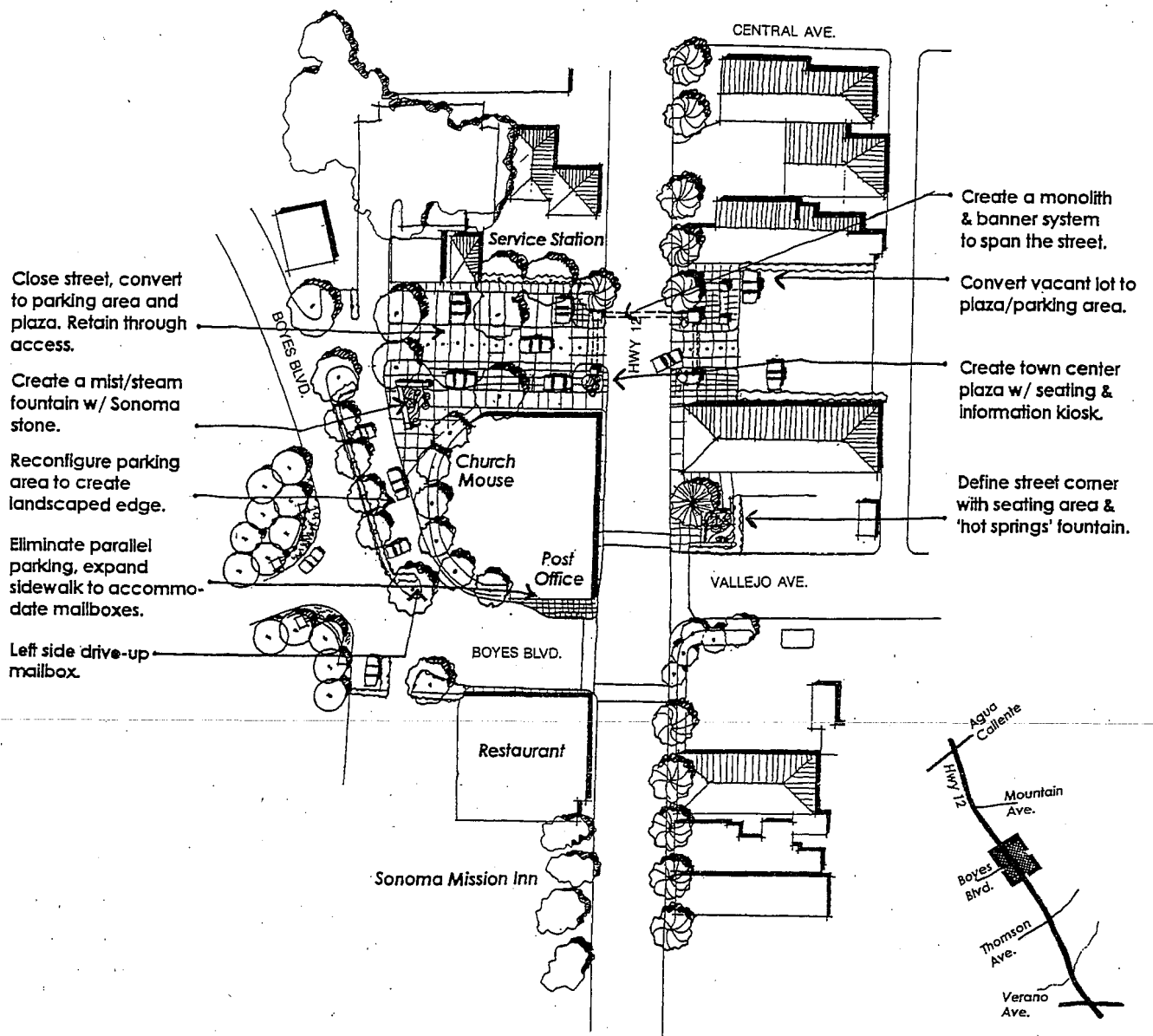
1. Create a "downtown" main street focus for the entire Springs Area.
2. Celebrate the special qualities, like the hot springs, Mediterranean setting and wineries.
3. Develop the area as a regional destination.
4. Develop further detailed design studies by retaining a consultant or by holding design competitions.

##### **Recommendations:**

1. Encourage development of pedestrian-oriented retail uses in this area.
2. Create off-street public parking areas to support retail use.
3. Celebrate the setting by incorporating hot mineral water and Sonoma stone into a series of fountains in the plaza areas. Provide informational signage describing the Spring Area history for tourists.
4. Convert Boyes Boulevard and Hwy. 12 intersections into a town square with seating, fountain and other amenities. Provide public parking in the plaza area.
5. Establish underground utilities and relocate mailboxes to improve the sidewalk quality.
6. Reconfigure the parking areas to create a landscape edge which defines Boyes Boulevard
7. Where possible, develop small plazas at corners for pedestrian use.
8. Develop a "Main Street" character with trees in grates fringing widened sidewalk.
9. Explore opportunities for a cable banner system across Hwy. 12 to promote public events.
10. Encourage merchants to enhance storefronts with pedestrian-oriented improvements such as awnings, blade signs, display windows, outdoor cafe, and recessed building entries.



*The Post Office is a major destination within the town center.*



## BOYES BOULEVARD STUDY AREA

### 3.4 AGUA CALIENTE / FETTERS HOT SPRINGS SEGMENT

The Agua Caliente/Fetters Hot Springs segment is more rural in character than other parts of the corridor. The roadway narrows, the sidewalk disappears, and the distinction between the road and off-street parking blurs. The predominant land use along this road is residential or service commercial. The intersection of Mountain Avenue and Hwy. 12 becomes an important node. This is a major pedestrian crossing for school children traveling to Flowery School. The vacant former hotel, located in the triangle of this intersection, is a significant landmark. The narrow setback of this two story structure creates a strong visual edge to the road which is reinforced by the steep embankment in the eastern edge of the road. The Curry & I Antique Store is also an architectural landmark in this segment of the corridor. The rural northern entry to the corridor is subtly defined by the Fire Station and the Eucalyptus and Oak groves along the street.

#### Mountain Avenue Study Area

##### **Issues:**

1. The dominant images consist of matured oaks, a strong architectural edge, and a cliff along the eastern edge. On the western edge is a random mixture of commercial uses with street parking merging with the Highway.
2. School children currently crossing Hwy. 12 at unsignalized areas raises significant concern.
3. The acutely aligned Keaton and Mountain Avenues, in relation to Hwy. 12, poses awkward vehicular ingress and egress.
4. The vacant building at the corner of Keaton is a visual landmark due to its historic architectural style and its dominant location.
5. The Agua Caliente historic monolith can be found in front of this vacant structure. There are no sidewalks in this area.

##### **Goals:**

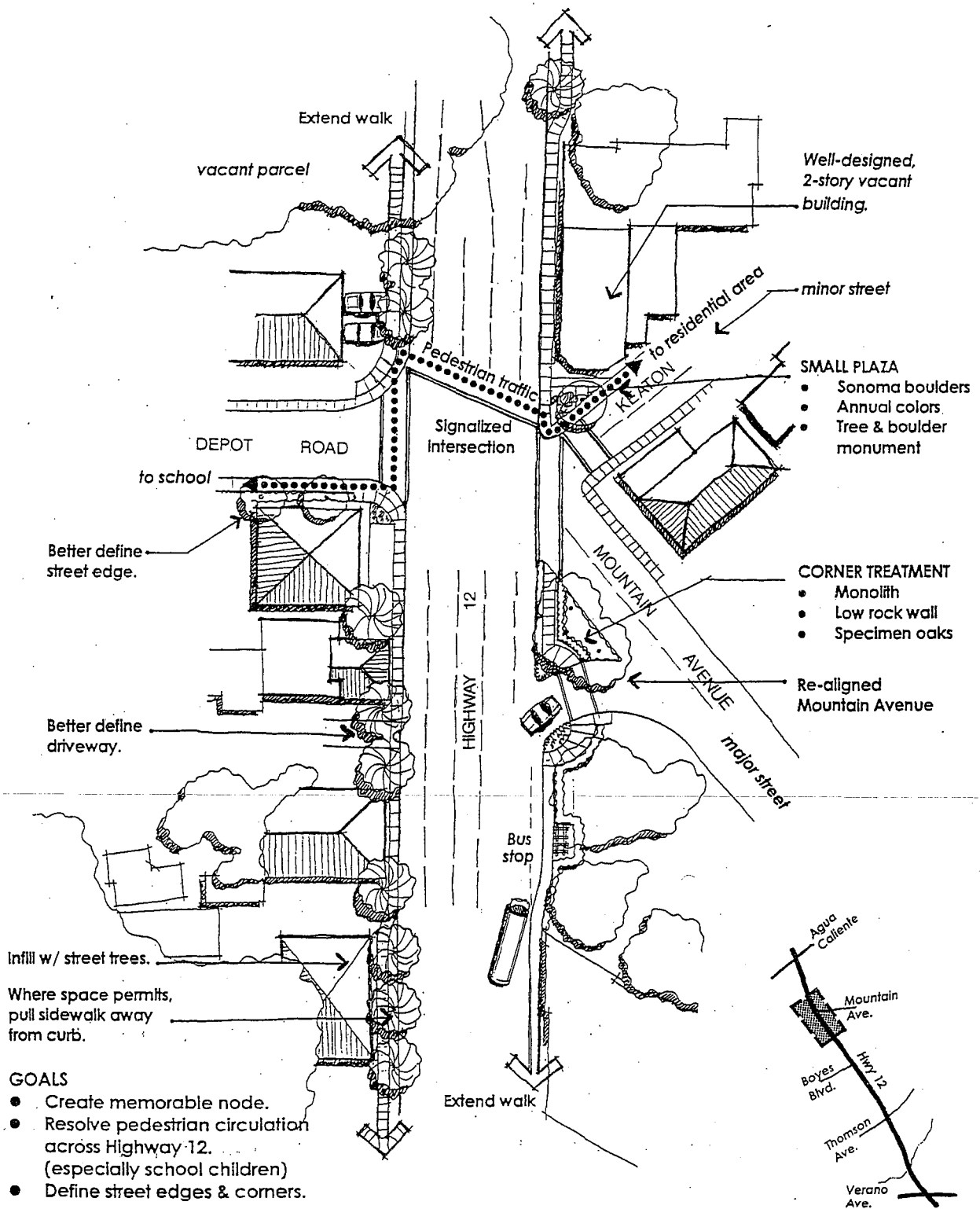
1. Create a memorable focus that provides an identity for the Agua Caliente community.
2. Provide a safe pedestrian crossing at Hwy. 12.
3. Resolve vehicular circulation conflicts.

##### **Recommendations:**

1. Simplify and reinforce the alignments of Keaton and Mountain Avenues for a safer intersection.
2. Create a right turn lane onto Mountain Avenue from Hwy. 12.
3. Utilize the triangle that resulted from road realignments to create a small landscaped island. Retain existing Agua Caliente monument in plaza area. Preserve existing oaks in the triangle and develop this area with special "springs" elements such as a low rock wall, special paving, flowering display, rock placements, or stone monoliths.
4. Provide pedestrian walks throughout the area. Create a clear circulation route for the school children. Where possible, sidewalks should be pulled away from the road to allow a planting area for street trees.
5. Provide an illuminated pedestrian signal across Hwy. 12.
6. Discourage parking areas which require vehicles to back onto Highway. 12. Consolidate driveways. Clearly distinguish pedestrian areas from auto circulation areas.
7. Encourage the use of landscaping along edges of buildings on Depot Road and at intersection corners to enhance the appearance of the area.
8. Create a bus stop with a characteristic bus shelter.



*Vacant building with historic architectural style.*



## MOUNTAIN AVENUE STUDY AREA



### 3.5 CREEK INTERFACE

There are two creeks within the Highway 12 Design Guidelines area. The larger creek, Agua Caliente Creek, is a major tributary of Sonoma Creek. Starting high in the eastern hills, Agua Caliente Creek is several miles in length; development along the creek starts only a 1/4 mile to the east of Highway 12. When not full from winter storms it serves as a pathway for animals from Sonoma Creek to the eastern wilderness areas.

The smaller creek originates in the eastern hills above the Pequeno Valle Ranch at the top of Park Avenue. It crosses Highway 12 south of the "Mountain Avenue Study Node" near Feters Avenue and flows into Sonoma Creek through Larsen Park. Although this creek has no formal name, it will be referred to as Pequeno Creek in this study.

#### Issues:

1. At bridge crossings, pedestrian and bicycle travel lanes narrow, bringing the users dangerously close to the vehicular traffic.
2. The creeks are not currently recognized as a positive natural asset along the corridor.
3. The destruction of riparian areas with adjacent future development raises concern.
4. Agua Caliente Creek is also a wildlife corridor in the dry seasons.
5. The existing bridge at Pequeno Creek has an opening approximately half the width of the winter creek flow.

#### Goals:

1. Provide safe pedestrian and bicycle bridge crossings at the creeks.
2. Create a highlight at creek locations.
3. Plan for creek preservation prior to future development as it is difficult to restore a riparian area after it has been disrupted.

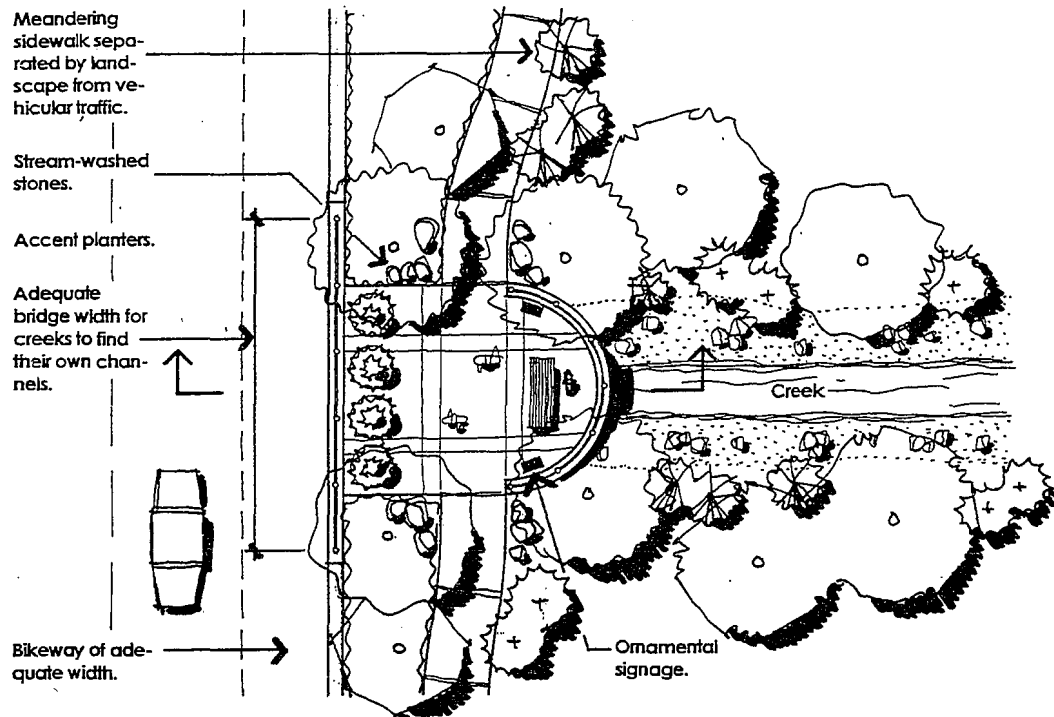
4. Ensure that new bridges have adequate openings for winter flow.

#### Recommendations:

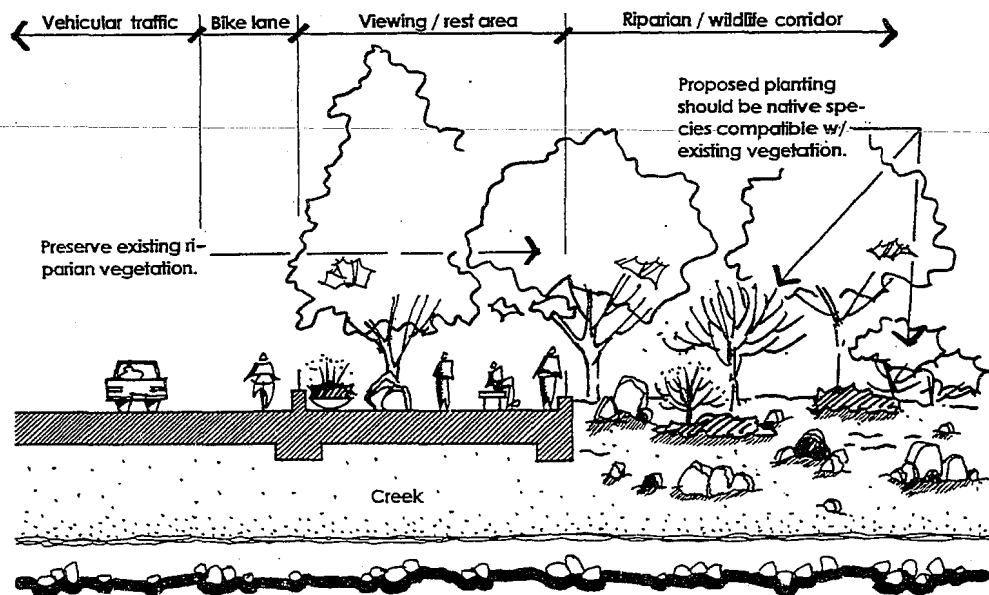
1. Include safe pedestrian sidewalks and bikeways of adequate width in the redesign of the bridges. Meandering sidewalks separated from vehicular traffic could be used at the creek areas.
2. Existing riparian vegetation should be preserved as far as possible. Proposed planting in adjacent properties should use native trees and shrubs compatible with the existing riparian vegetation to retain and reinforce the creek and wildlife corridor. Unnecessary and indiscriminate removal and replanting should be avoided.
4. Create special viewing areas by providing benches, shade and accent planting at the creek locations. Ornamental signage with historic information could be included in the design. Finishes may include informal placements of stream-washed stones.
5. The redesigned bridges should, as far as possible, provide adequate width for creeks to find their own natural channel.



*Caption.*



## PLAN



## SECTION

### LOOKOUT AT CREEK

### How to use the Design Guidelines

These guidelines contain special design criteria for development along the Hwy. 12 corridor and should be used as a supplement to Sonoma County Zoning, design standards, and guidelines.

The Design Guidelines have been divided into three categories to reflect the three predominate land use characters found along Hwy. 12. They include:

1. Main Street
2. Country Commercial
3. Corridor Residential

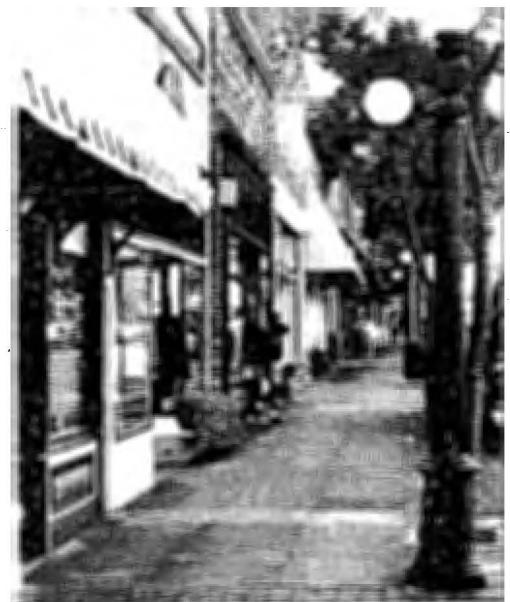
The **Main Street** designation refers to those areas along the corridor where a concentration of pedestrian-oriented retail stores are clustered to create a shopping environment similar to the "Main Street" atmosphere of a rural town. Appropriate uses in this area might include a bookstore, antique shop, boutique, bakery, delicatessen, cafe or other uses which are oriented towards pedestrian traffic. Thus, the scale of the architecture and signage is oriented towards this pedestrian use, and a continuous row of store front shops line the sidewalk. Customers typically park in a public lot or rear private lot and visit a number of shops in a single trip. Residential or office space located above shops can contribute to overall vitality. Restaurants, theaters and other night time uses are encouraged in this area.

The **Country Commercial** designation refers to the balance of the area along the corridor zoned for commercial uses. Typical uses in this area might include professional and medical offices, auto services, hardware stores, furniture stores, nursery, day care center or other uses that typically occur in a free standing building accommodating its own parking area on the same parcel. The customers arrive primarily by car. Thus, the building entry although visible from the street is typically oriented to provide convenient access from the parking area. The scale of development reflects this auto traffic orientation and the landscape setback is used to create a visual continuity between the individual buildings.

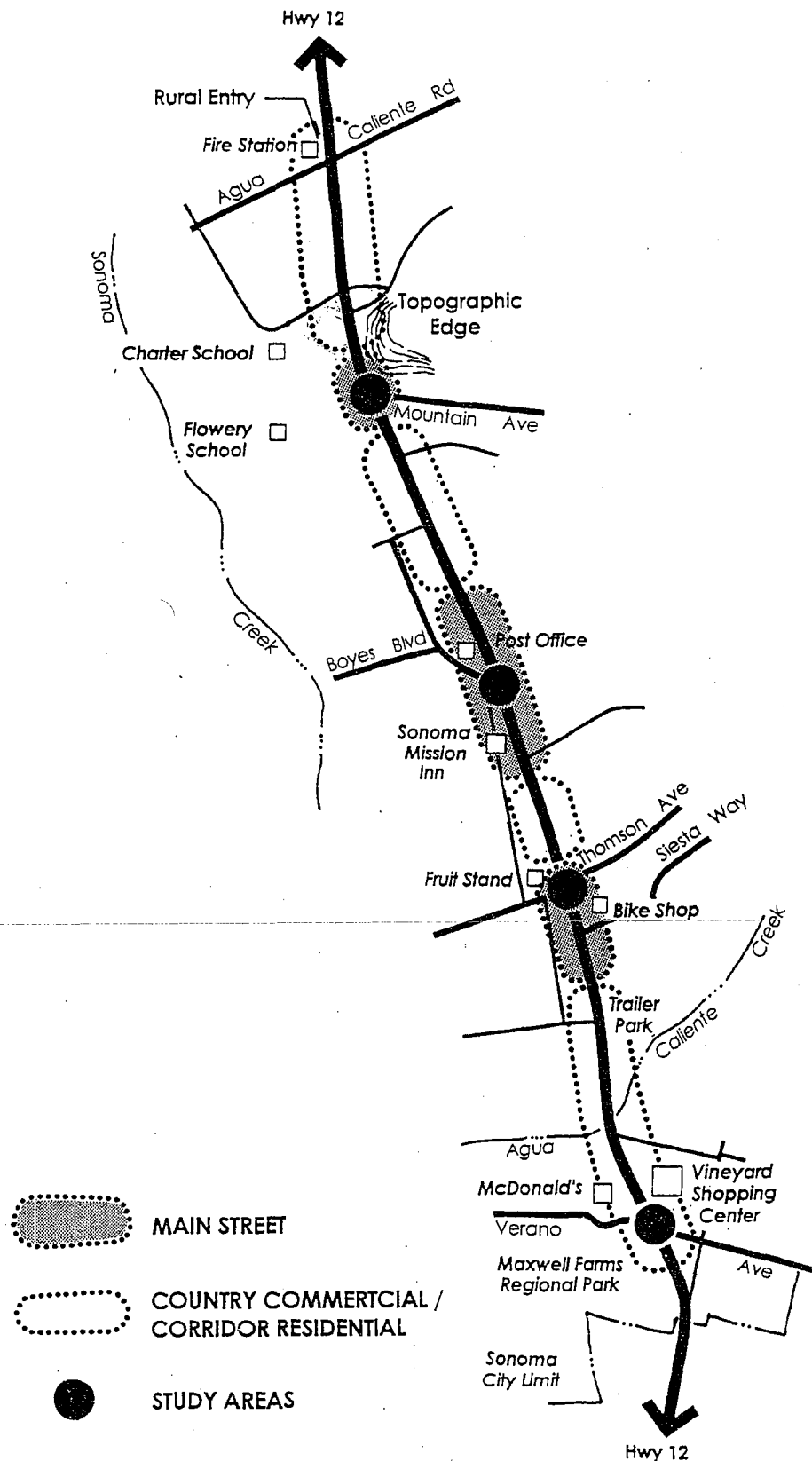
The **Corridor Residential** designation refers to any residential property (multi-family or single family) which abuts Hwy. 12.

A property owner should refer to the following illustration showing the general location of Main Street and County Commercial uses along the corridor to determine which guidelines are most appropriate for a specific parcel. Corridor Residential can occur in any County Commercial area where zoning permits a residential use. The black dots on the map indicate the study areas along the corridor. Property owners should refer to the conceptual plans of these study areas shown in the corridor overview portion of this document for detailed information on proposed streetscape improvements in these areas.

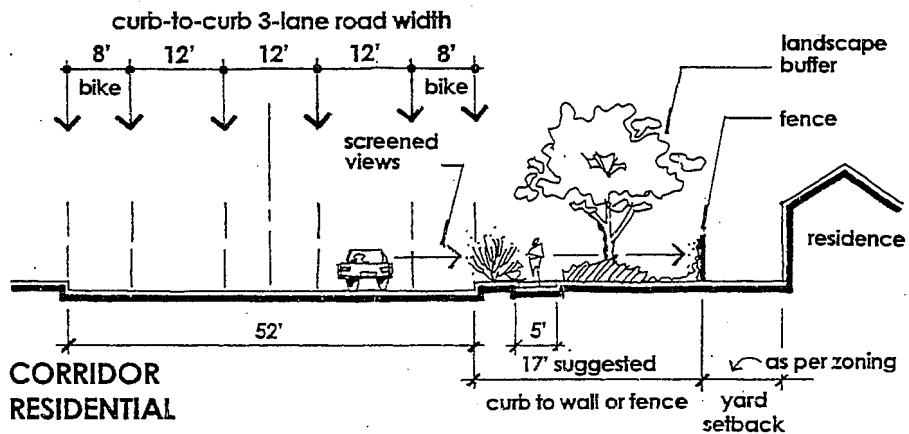
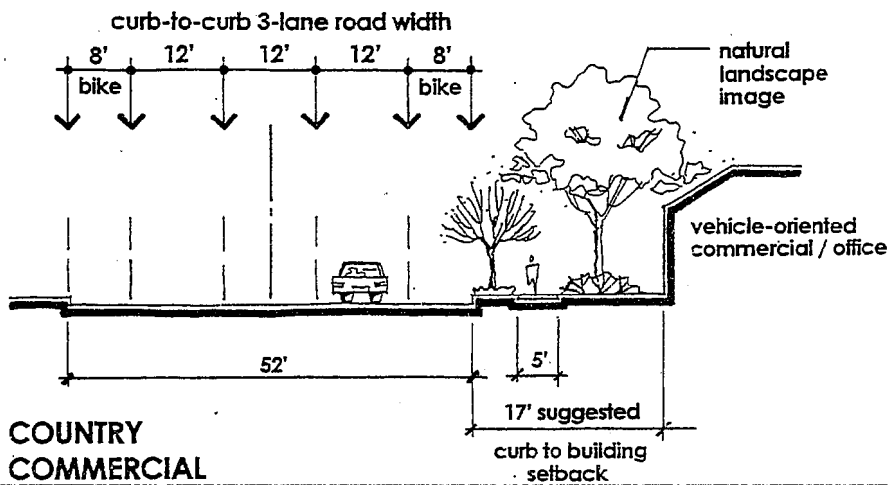
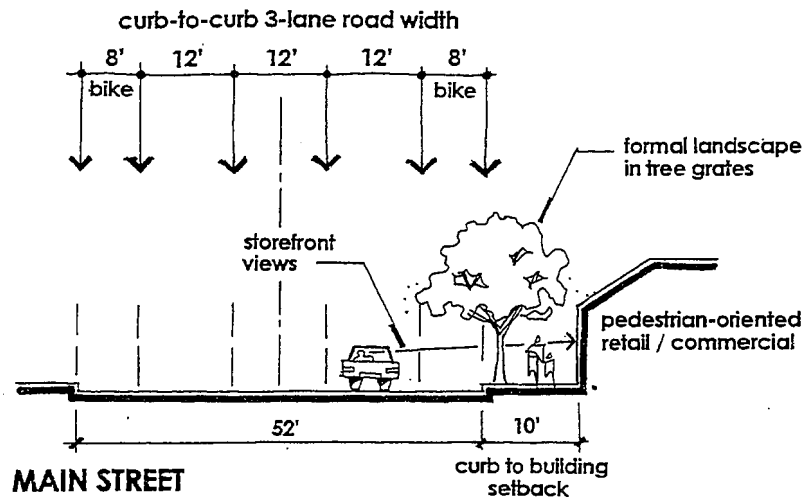
For specific information on types of street furniture, plant materials or screening, refer to Chapter 7 Site Elements Appendix. This chapter also includes general information which supplements the specific guidelines under the three land use characters.



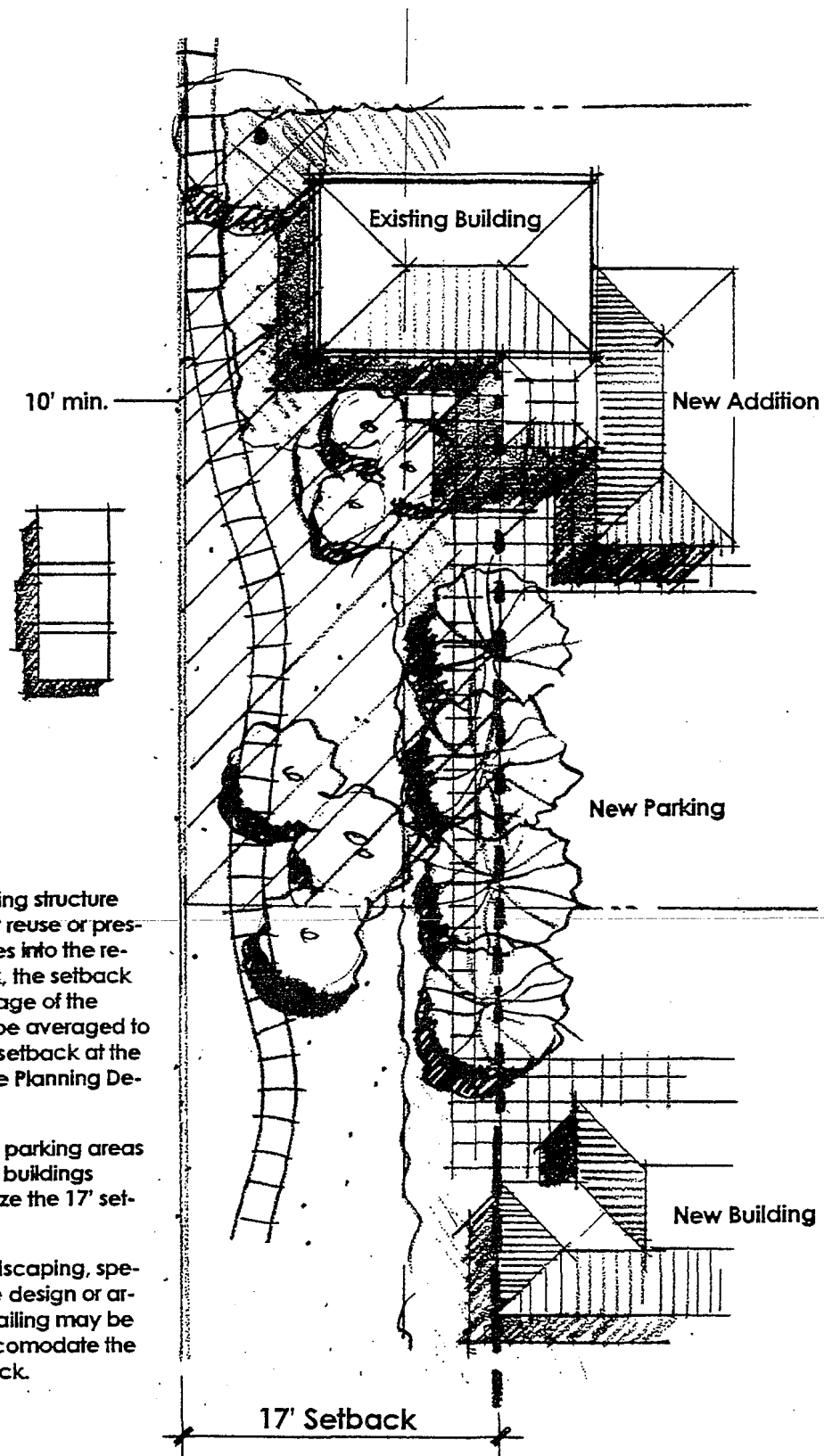
*Main Street atmosphere of a rural town.*



## LAND USE TYPE



## STREET SECTIONS & BUILDING SETBACKS



Where an existing structure appropriate for reuse or preservation intrudes into the required setback, the setback along the frontage of the property may be averaged to maintain a 15' setback at the discretion of the Planning Department.

New additions, parking areas and additional buildings should recognize the 17' setback.

Additional landscaping, special hardscape design or architectural detailing may be required to accommodate the reduced setback.

## AVERAGE BUILDING SETBACK CONCEPT

***MAIN STREET***

Chapter 4

MAIN STREET

main street

*main street*



## 4. Main Street

### 4.1 SITE PLANNING

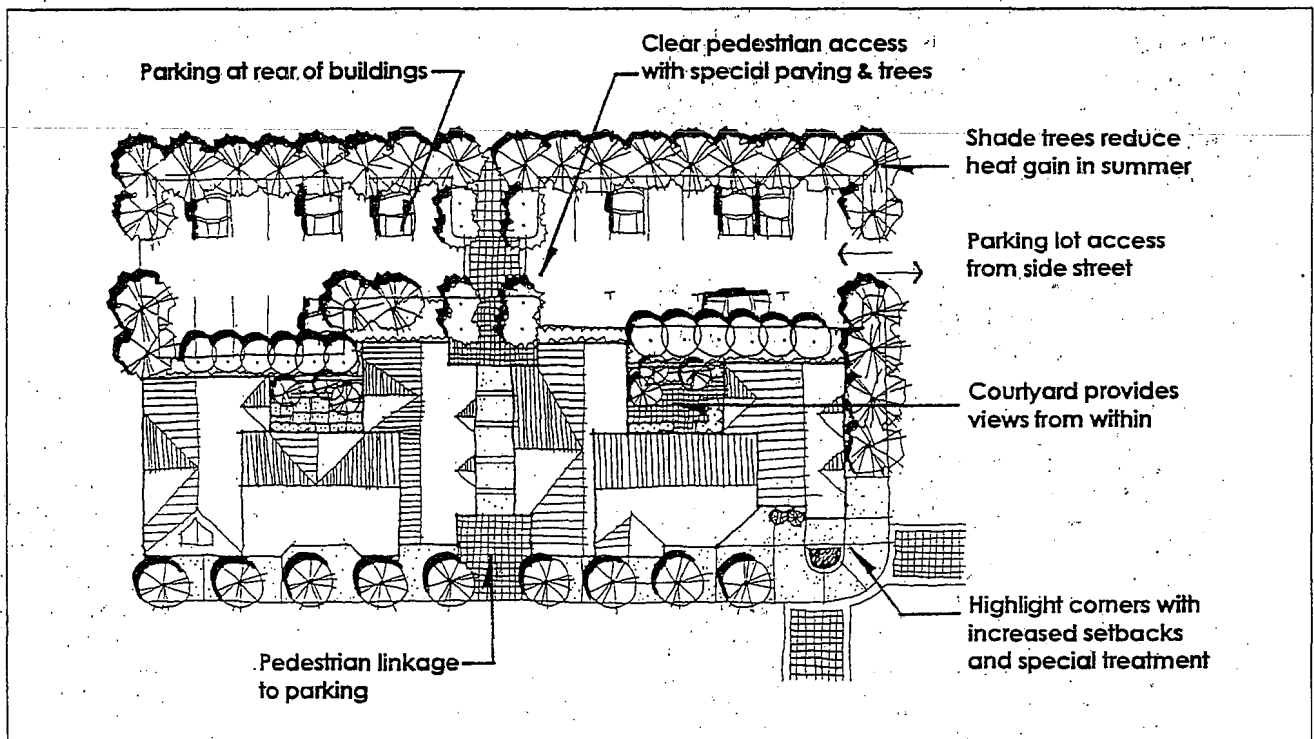
The goal of these guidelines is to create a pedestrian-oriented, downtown atmosphere within the existing development pattern.

#### A. Right-of-Way Treatment

1. Buildings should be at a setback of a minimum of 10 ft. from the face of the curb, or 36 ft. from the centerline of the street, whichever is greater.
2. Provide a 10 ft. wide sidewalk. Enriched paving may be used on the sidewalk to highlight entries to the building.
3. Street trees should be located with tree grates at 20 to 25 ft. on center. Locate trees to minimize conflicts with signage visibility.
4. The streetlight in the right-of-way should be the Sternberg traditional single light on posts. (See Appendix).

#### B. Vehicular Circulation & Parking

1. The predominant views from the highway should be of architectural storefronts. Therefore, locate the majority of parking at the rear of the site. Explore opportunities to consolidate rear parking areas between multiple parcels through use of cross easement areas, and create public parking lots. Increase public access to private lots through space leases, voucher or parking district arrangements.
2. Where possible, access to rear parking should be provided from side streets or alleys to avoid curb cuts on Hwy. 12.
3. Ample parking should be planned with adequate designated employee parking to avoid conflicts with customer parking.
4. Parking areas should be designed such that a car will not be required to enter a street to move from one location to any other location within the parking area or premises, with the exception of designated employee parking.



*Conceptual rear parking with side street access.*

5. Develop secondary pedestrian entrances to retail uses from these parking areas.
6. Clearly indicate off-street parking areas with thematic signage along Hwy. 12 to encourage use of rear parking areas.
7. Curb cuts on corner lots should be located at the farthest point away from the curb return as is feasible.
8. All parking lot area not needed for parking, driveways or pedestrian access should be landscaped.
9. Provide landscape buffer between rear parking areas and adjacent residential development.
10. Bicycle racks should relate to building entrances and pedestrian walkways. Locate bike parking in areas where surveillance is possible.
11. Due to constrained situations in the Main Street area, it may be necessary to work with adjacent parcels to create an integrated drainage system to resolve parking lot drainage.

### C. Pedestrian Circulation

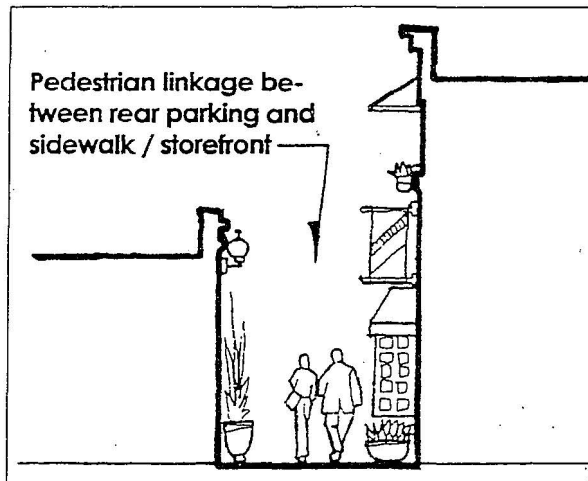
1. Main building entries should be directly accessible from the sidewalk.
2. Design pedestrian linkages between rear parking areas and sidewalks in such a fashion as to encourage use of parking areas. Thematic portals, lighting, paving and planting should be used in the linkages. Retail uses should orient display windows and entries onto pedestrian linkages.
3. Provide pedestrian amenities such as benches, trash receptacles, planters, etc.
4. Enhance pedestrian activities along the street by providing wider sidewalk areas and recessed entries for outdoor seating or cafe use.
5. Encourage use of arcades, awnings and overhangs which protect pedestrian circulation areas.



*Storefronts with entries directly accessible from the sidewalk.*



*Signage incorporated into the overhead structure at the pedestrian linkage between the rear parking and the sidewalk.*



*Pedestrian linkage between rear parking and storefronts.*



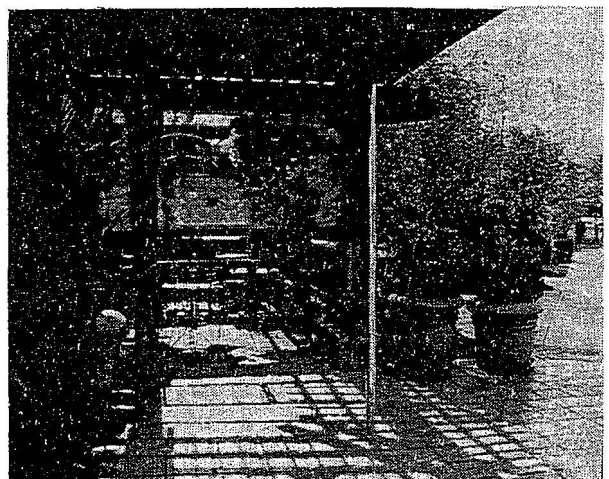
*Linkage with arbor, special paving and secondary entries.*



*Vines on trellis with arbor create a welcoming atmosphere.*



*Lush landscape with grape trellis shades outdoor eating area.*



*Widened sidewalk with spillover eating area.*

#### D. Service Areas

1. Where possible, property owners are encouraged to consolidate their trash storage, utility and loading areas.
3. Interior parcel: The service area is restricted to rear yards.
4. Corner parcel: The service area should not extend beyond the rear two thirds of the building length on one side yard, and should be screened from the adjacent street.

#### E. Fencing

1. Where adjacent to residential areas, a 6 ft. high solid fence or wall is required.
2. Screen parking with low walls, fencing and landscape where visible from Hwy. 12 or side street.
3. Fencing between parcels of similar uses is discouraged; doing so tends to disrupt cross circulation.



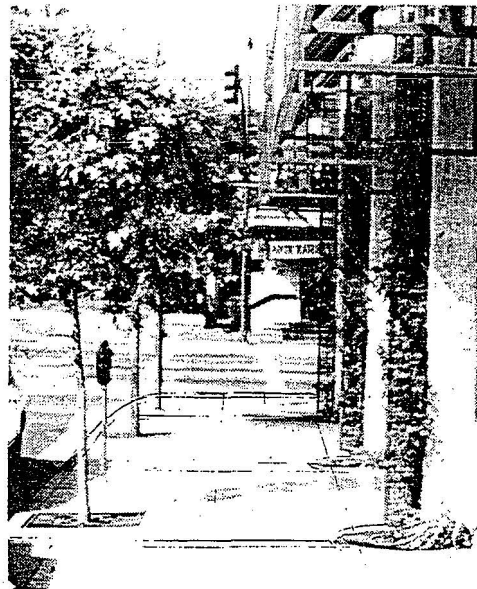
*Dry stacked low wall is a fencing option.*

#### 4.2 LANDSCAPE & SITE ELEMENTS

In the Main Street area, there will be less reliance on plant material as compared to decorative pavings and site furnishings. The trees will be formally spaced in tree grates along the sidewalk, which creates a manicured setting. Spillover and visibility of store displays will generate liveliness on the sidewalk.

##### A. Landscaping

1. Use landscaping to enhance pedestrian areas. Include trellises and arbors with vines along building facades, pots with annual colors at building entries, and flowering patio trees in courtyards.
2. Parking entries should be enhanced with special plantings.
3. Where possible, provide canopy shade trees in the rear parking areas adjacent to a street.
4. Motorist and pedestrian views of long expansive building walls or paving should be visually broken with appropriate landscaping.



*Expansive walls & paving broken with landscaping.*

## B. Street Furnishing

1. The provision of pedestrian amenities such as benches, pots, trash receptacles, pedestrian lighting, and special sidewalk paving is strongly encouraged. Street furniture should be clustered to create special seating areas.
2. Pots should be used to add rhythm and punctuation to the streetscape and pedestrian routes. A variety of pot sizes and heights should be clustered for an interesting composition.
3. Tree grates should be used in areas of high pedestrian circulation.
4. Special paving and landscaping should be used to enhance pedestrian paths, plazas, building entries and intersections.
5. Newspaper stands, telephone booths, automatic teller machines, and other assorted mechanical fixtures should have a subdued appearance that is integrated into the architecture, and not appear like an add-on. Such attachments should be unobtrusive with no added signage and colorful features.

## C. Lighting

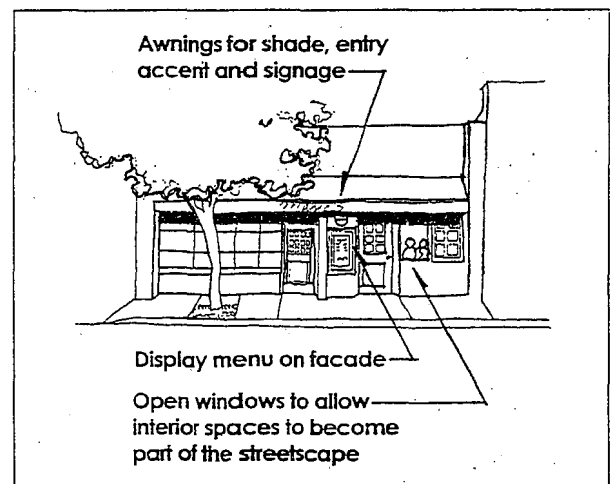
1. Lighting in this area should focus on the pedestrian circulation, building entries and window displays.
2. Encourage creative illumination such as building mounted bracket lights, tree uplights under tree grates, bollards along pedestrian linkages, and ornamental light fixtures to create effective imageries for evening use.
3. Pedestrian sidewalks should utilize Sternberg light fixtures (See Appendix). Banners could be incorporated onto the light poles for added color and liveliness.

## 4.3 ARCHITECTURE

A continuous storefront facade is the image of the Main Street architecture. The predominant building finishes should be stucco, reflecting a Mediterranean style, and quality painted wood siding with historic detailing. Rich detailing at the pedestrian level will contribute to creating a pedestrian-friendly environment.

### A. Front & Rear Facades

1. New buildings should relate to the existing architectural styles. The height of buildings, moldings, awnings, signage and other architectural elements should bear a relationship to elements of adjacent existing structures. There should be a general continuity of glaze lines, soffits, etc. While owners are encouraged to create interesting new architectural styles, they should not contrast unfavorably with existing roof lines and facades.
2. Establish a consistent storefront edge along the street by locating buildings directly at the back of the sidewalks. The exceptions should be at recessed entries or notched corners where pedestrian eddy spaces or plazas may occur. Minimize the side yard setbacks to maintain a continuous architectural edge from parcel to parcel.



*Favorable front facade elements.*

3. Large, blank, prominent exterior surfaces should be avoided. Walls facing pedestrian ways, alleys and parking should have intricately designed architectural detailing, signage, awnings, or landscaping.
4. Roofs should be relatively flat and be hidden by parapet walls. Street facade parapets should be simply shaped and projecting cornices with architectural moldings are encouraged. Upper level windows should have shapes, proportions and rhythms similar to those on adjacent buildings.
5. Allow a variety of building heights and details, while establishing a consistent horizontal element at display windows, awnings and doorways.
6. Encourage buildings to be two stories, or one story with cornices that heighten the appearance of the architecture. The maximum height should not exceed two stories.
7. Buildings should utilize compatible materials on all sides. Emphasis should be placed on the detailing of facades which face the street. Enhance public rear entries directly accessible from the parking areas.
8. Screen trash and storage areas in the rear of the building with permanent enclosures integrated into the architecture of the building. Eliminate tacked-on sheds.

#### **B. Entries & Window Displays**

1. Offsets or notches in building facades are encouraged to highlight entries directly accessible from the sidewalks.
2. Provide an attractive secondary pedestrian entrance from the rear parking lot.
3. Rounded, geometrically shaped or angled corners with entries are encouraged for corner buildings.
4. Side walls of buildings adjacent to pedestrian paths should incorporate interesting display windows and secondary entries.
5. Lattice screens or large operable windows, (i.e., sliding or rolling) should be utilized by restaurants and other similar uses to impart an outdoor cafe atmosphere which enhances the quality of sidewalk activities.
6. Multi-paned windows and specially trimmed doors should be located at the entry facades to enhance the human scale and add interest.
7. Large, attractive window displays should remain lit in the evening to create visual interest, security and an atmosphere for evening window shopping.



*Attractive window displays add to the scale and liveliness of the sidewalk.*

### **C. Awnings**

1. The design of awnings should enhance the visual aspects of the streetscape, especially when viewed from a pedestrian scale. They should be an extension of, or embellishment to the architectural design of the building and should not appear as an add-on element.
2. Each storefront should have its own awning on a separate frame and in a color which is complementary to the color palette chosen for the building.
3. Awnings may extend into the front setback.
4. The location of an overhang or awning should respect other architectural elements, and should not hamper the signage potential of the building.

### **D. Color & Material**

1. All materials and finishes should reflect the regional and historic character of the site.
2. The color scheme should unify the streetscape and buildings. Peach, rose, grey and beige hues should be the main colors for the new thematic palette. These colors could be utilized in paving materials, street furniture, and the exterior finishes.
3. Each building should use a maximum of four shades or colors which complement and relate to each other. If there is an existing unpainted material like brick, this material should become one of the chosen colors. Whenever paint is used, it should be superior in quality and graffiti-resistant.
4. Building colors should be compatible. Each color must not be such that it competes for attention. Building colors should be subdued and not garish, and should not in any way become the "signing" of the building or site.
5. The darkest color chosen from a color palette should be used at the building base to establish a perceived 'anchor' for the building. The lightest color should be used on the majority of the building. In general, main building colors should be light in tone; off-whites, light greys, light beiges and pastel colors are preferred. Dark colors tend to make a building appear larger, more massive and less detailed. A third contrasting color should be used to highlight trims such as fascias and window frames. This trim color can be either lighter or darker than the main color. A fourth color can be used to highlight additional architectural features, such as mullions, cornices and awnings.
6. Smooth stucco finishes should be used as the primary wall surface material on all building facades. Brick may be considered, but should be used only where it would relate to an adjacent building. Tile bases in historic colors and patterns are encouraged. Craftsman quality wood finishes and detailings are also appropriate.
7. Construction materials at the building base should be consistent with its own, as well as adjacent facades. These materials should not detract from the existing details of the building.
8. Use materials which are easily maintained, long lasting and add distinctive architectural character from a distance (e.g., brick, intricately-detailed stucco and cut stone of muted tones).
9. Mirrored glass or heavily tinted glass should not be permitted. Mirrored glass frequently gives off blinding reflections and heavily tinted glass appears dark and foreboding and adds little life to the street. Clear glass provides visual access to the window displays and interior activities.
10. Materials, textures, colors, and patterns should be expressed as an integral part of the architectural concept. Elements that look superimposed are not desired.

### **E. Building Rehabilitation**

1. Buildings should be cleaned and repainted. Existing ornamental cornices, parapets, and other architectural details should be preserved, repaired, or replaced to match the original.
2. Materials which have been added to the facade (i.e., stone, brick, tile, wood shingles, etc.) since its initial construction, and which are not consis-

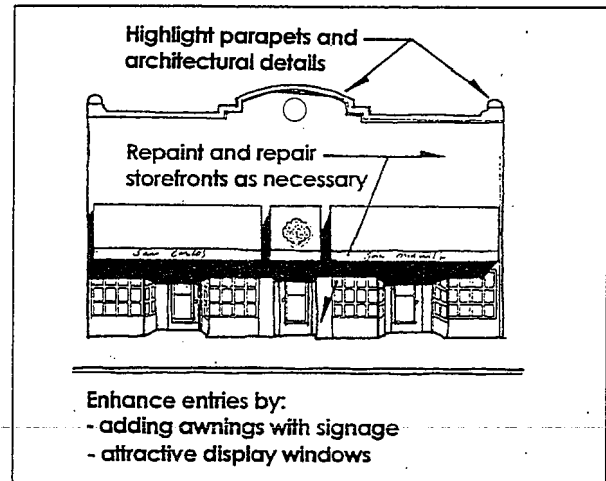


tent with the original design, should be removed and replaced with materials which are compatible with the historical character of the building and adjacent structures.

3. Canvas awnings should always be kept fresh and replaced if necessary. Deteriorating, semi-permanent awnings or canopies attached to the facade should be removed.
4. The replacement of natural finish aluminum sections with anodized aluminum, vinyl coated metal or wood frames is encouraged. In doing so, consideration should be given to create more open and inviting facades.
5. Replace deteriorating window frames. New windows should be architecturally consistent with the facade character.
6. Old signs, which are worn and faded, or out of scale with the character of the building should be replaced, or if of historical value, should be repaired.
7. All exterior air conditioning units, exposed pipes and conduits, old brackets, and other elements which clutter the building appearance should be removed.
8. Follow appropriate laws, codes and ordinances when improving any building, and include the necessary elements and details to ensure a barrier-free design.



*Buildings should be cleaned and repainted.*



*Rehabilitation ideas.*

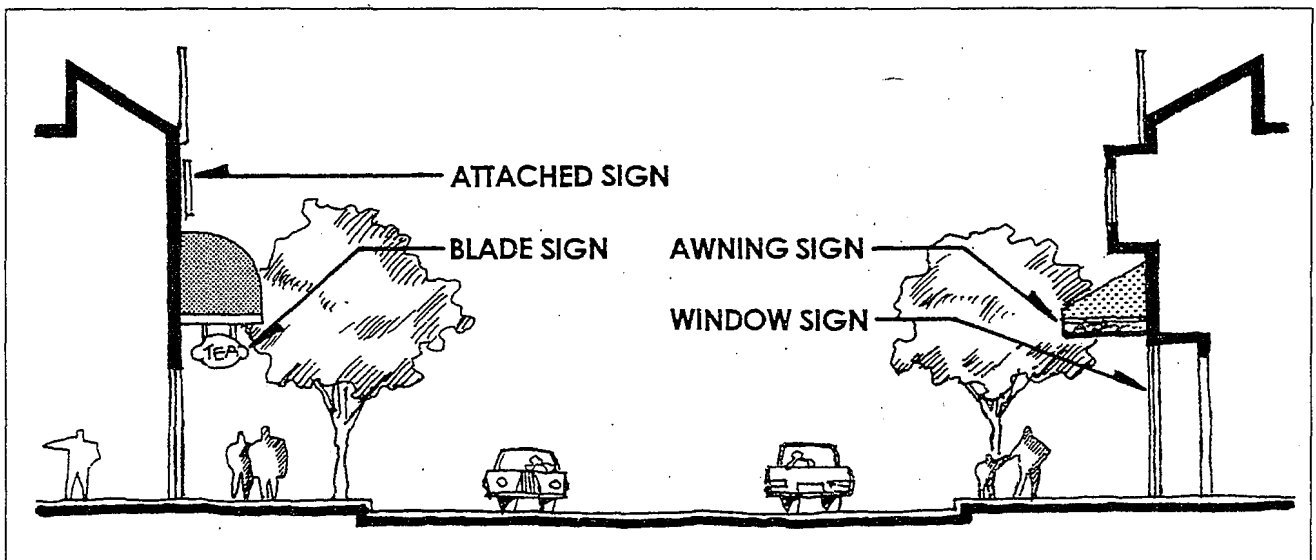


*Building with good potential for rehabilitation.*

## 4.4 SIGNAGE

### A. Attached Signs

1. Integrate attached signs into the architectural elements or details. Signs should be of an appropriate scale for the building facade.
  2. All storefronts are permitted to have two permanent signs. One on the front facade, such as a wall sign or awning sign, plus a pedestrian-oriented projecting blade sign or window sign. Total area of both signs should not exceed 32 sq. ft.
  3. Roof signs or signs which extend above the parapet line of a facade should not be allowed with the exception of the theater marquee.
  4. Any building with a secondary entrance adjacent to a street, a pedestrian alley or an off-street parking area should be allowed to have an attached wall sign in each of those locations. These signs are not to exceed 16 sq. ft. in area.
  5. Freestanding signs should be discouraged except as directional signs for automotive purposes (e.g., "exit only" or "to parking", see appendix.)
1. Wall signs which are mounted or painted directly on the wall are typically used to identify businesses to passing motorists or pedestrians across the street. Thus, the sign should be sized for these viewers.
  2. Wall signs may include individual solid letters, individual internally illuminated letters, a board sign (i.e., individual letters displayed on a flat surface which is then mounted on the building face) or lettering painted directly onto the building face.
  3. Boxed or cabinet display signage (i.e., a metal frame containing internal illumination and a facing material containing the lettering) should not be permitted.
  4. All wall signage should fit within a "Signable Area" on the wall surface uninterrupted by doors, windows or architectural details. The "Signable Area" should not exceed 20% of a building facade.
  5. The individual letters, attached board sign or boxed sign should not exceed 2/3 of the height of the "Signable Area" or 18 inches in height.



## BLADE SIGNS

This type of signage is associated with pedestrian-oriented shopping areas and traditional downtown main streets. The blade sign is a way for the merchant to create and display the symbol which illustrates their business and/or service. As the signs are mounted perpendicular to the wall surface with both sides visible to the public, the sign must be dual faceted.

1. Blade signs should be conditional upon County design review for exceptional quality.
2. The signs should be carefully located so that they are clearly visible to pedestrian traffic and are not obscured by trees or awnings.
3. Each ground floor business may be allowed one blade sign which may project over the public right-of-way. Signs should extend no more than 36 inches from the face of the building at sidewalk. They should be mounted to allow a minimum of 8 ft. of clearance.
4. Sign supports should be well designed and compatible with the character of the sign.
5. Interior illuminated boxed signs should not be allowed. Exterior lighting of the blade signs should be encouraged.
6. Blade signs should fit within an imaginary rectangle with a maximum area of 6 sq. ft.
7. Icon signs using shapes or symbols uniquely suited to the business, creative shapes and three dimensional signs should be encouraged.

## WINDOW SIGNS

Window signs are primarily oriented to the passing pedestrian.

1. The signs may consist of:
  - a) Individual letters and/or logo etched, sandblasted, or painted directly onto the window,

- b) Vinyl letters applied to the glass,
  - c) Individual, three dimensional, internally illuminated, backlit (halo), metal letters mounted inside the window, or
  - d) Neon with historical character.
2. The color scheme and design should complement the storefront and window display.
  3. Window signs should be restricted to store name or logo, services offered and operational information such as store hours and credit cards accepted.



*Blade signs with icons and logos.*

Signs for product information and brand names should be discouraged. No product advertising or logo placards, banners, pennants, insignias or trademarks or other descriptive material should be affixed or maintained upon the storefront or in display windows.

4. Window signs should only be allowed in ground floor windows.
5. The maximum allowable total area of window signage should be 10% of the aggregate ground floor window area, not to exceed 25% of any single window area. The sign should not obscure views into the store or display area.
6. Temporary signs and posters (e.g., coming community events) should be limited to not exceed a total of 25% of the aggregate ground floor window area excluding doors. These signs should not be allowed on entry doors. Signs should be grouped to avoid blocking views into the stores.

#### SIGNAGE ON AWNINGS

Signage can occur on both the awning canopy or the valance. Letter height on awnings or valances should not exceed 8 inches in height. The use of logos or symbols depicting the unique nature of a business are encouraged on the sloped face of awnings. The image area should not exceed 10% of the total sloped awning face.



*Window sign: Vinyl letters on glass.*

Reinforce storefront edge across parking areas w/ dry stacked stone wall/low fence & perennial plantings.

Provide bike racks.

Thematic signage identifies public parking areas.

Minimize driveways/ curb cuts interrupting sidewalk. Link parking access to side streets.

Notch buildings to create entry & display alcoves.

Encourage outdoor displays & cafe use along sidewalk.

Signs on building parapets, awnings, or blade signs.

Store entries directly accessible from sidewalk.

Use formal pattern of street trees in free grates, benches and trash receptacles to create a Main Street pedestrian atmosphere.

Create an informal parking area. Provide canopy trees to shade & enclose parking. Encourage outdoor display and cafe use along the sidewalk.

Cross easements in the rear area are used to create shared parking and service areas.

Preserve existing trees where possible.

Buffer residential areas w/ 6' fence & evergreen trees.

Create pedestrian linkages between rear parking areas & sidewalk. Orient display windows or shop entries to linkage. Highlight linkages with thematic arbors.

Service areas integrated into architecture.

• Use pedestrian scale ornamental light to highlight Main Street areas. Light fixtures could incorporate seasonal & festive banners.

• Encourage entries & window displays along sidewalk. Avoid long blank facades.

Highway 12

## MAIN STREET

***COUNTRY COMMERCIAL***

COUNTRY COMMERCIAL

country commercial

*country commercial*

Chapter 5

## 5. Country Commercial

The goal of the following guidelines aims to retain the rural country character while providing for viable commercial uses.

### 5.1 SITE PLANNING

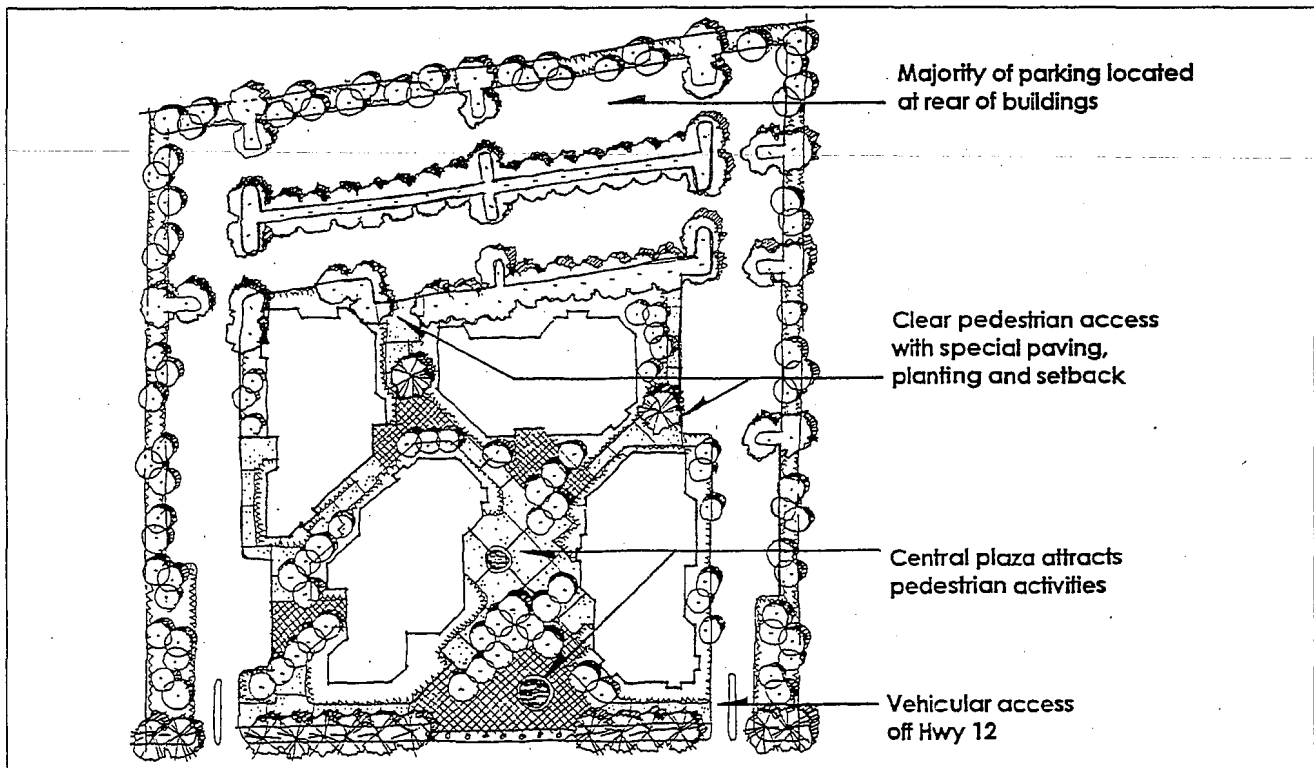
#### A. Right-of-Way Treatments

1. Buildings should be at a setback of a suggested 17 ft. from the face of the curb. Where an existing building intrudes into the setback, the frontage of the property may be averaged to maintain a 15' setback at the discretion of the Planning Department.
2. Provide a meandering 5 ft. minimum concrete sidewalk separated from the curb by a minimum setback of 8 ft.
3. The setback should be landscaped with natural clusters of trees, shrubs, groundcover and gentle berms which blend with the rural setting. Provide trees at an average of 20 ft. on center within the setback.

4. The streetlights used within this right-of-way should comprise of Davit poles with Cobra head fixtures. Restrict lighting levels to the minimum necessary for safety.

#### B. Vehicular Circulation & Parking

1. Locate parking to the rear and sides of the building.
2. Driveway throat distance of 15 ft. minimum should be provided between the property line and the first parking stall to allow vehicles to enter and exit the public street without interference with the parking spaces.
3. Curb cuts on corner lots should be located at the farthest point away from the curb return as is feasible to avoid potential traffic conflicts.
4. Parking areas should be designed such that a car will not be required to enter a street to move from one location to any other location within the parking area or premises.



*Conceptual side and rear parking.*



5. Required parking area landscaping should be located to reinforce primary circulation pattern. All areas not needed for parking, driveways or pedestrian access should be landscaped.
6. Locate bike parking in areas where visual surveillance is possible.
7. Parking aisles should be oriented to allow direct access to building entrances.

#### C. Pedestrian Circulation

1. Create a clear, safe pedestrian connection between parking areas and building entries.
2. Highlight building entries with plazas. Provide pedestrian connection between sidewalks and building entries.

#### D. Service Areas

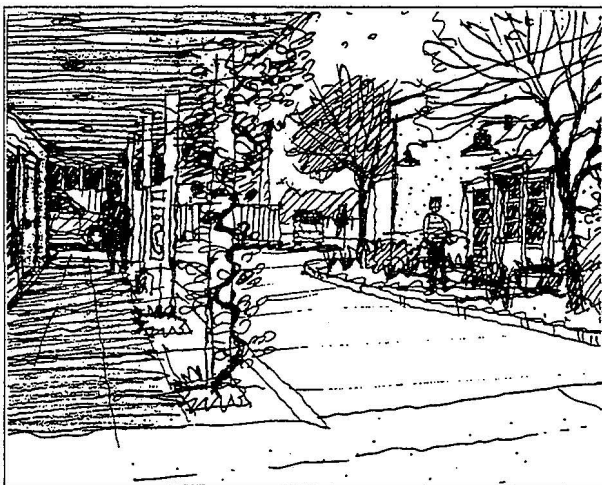
1. Where appropriate, property owners are encouraged to consolidate their trash storage, utility and loading areas. Avoid locating the service area where it impacts adjacent pedestrian circulation.
2. Interior parcel: The service area is restricted to rear yards or rear portion of side yards. No more than one third of building length should be for service yard use.



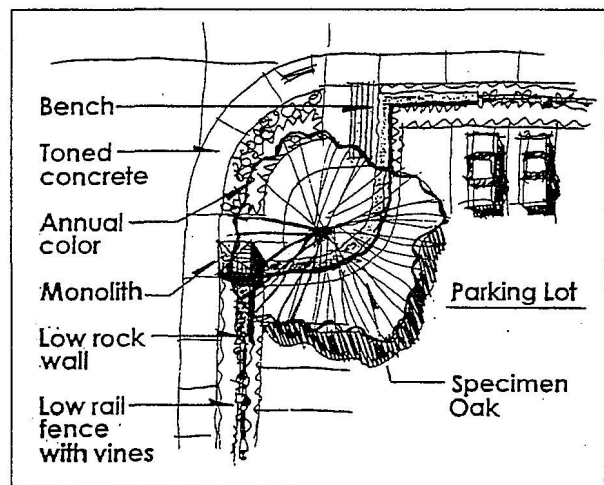
*Landscaping reinforces parking and circulation pattern.*



*Clear connection between parking and building entry.*



*Shared driveway access to reduce curb cuts.*



*A corner not used for parking is landscaped with seating.*

- 
3. Corner parcel: The service area should not extend beyond the rear two thirds of the building length on one side yard, and should be screened from the adjacent street.
  4. Service areas must be screened from public streets, adjacent parcels, as well as vehicular and pedestrian traffic areas with a 6 ft. high masonry wall or solid wood fence, and landscaping which is compatible with the architectural character.

#### **E. Fencing**

1. Parking should be screened by low walls, landscaping, and berms where visible from Hwy. 12 and side streets.
2. Fencing between parcels is not required unless it is used to mitigate land use or visual conflicts.
3. Chain link fences should not be permitted where visible from Hwy. 12.
4. Where fencing is required, it must be compatible with the architectural character of the development.
5. When abutting a residential property, screen the parking area with a 6 ft. high solid fence or wall.

#### **F. Lighting**

1. Illumination should focus on architectural elements rather than the streetscape.
2. Encourage creative architectural lighting to articulate building elements such as towers, decorative cornices and columns.
3. Use dramatic landscape lighting to highlight trees and planting compositions.
4. Lighting fixtures used in this area should be compatible with the overall country setting.

## 5.2 LANDSCAPE & SITE ELEMENTS

The landscape should reflect an informal country setting with agrarian themes such as oak clusters and grape arbors. When a more structured landscape is desired, agrarian references such as remnant orchard grids and vineyard rows may be used. Special riparian plantings will be used to highlight creeks and dry swales. Split-rail fences and stone walls are appropriate fence images within this area.

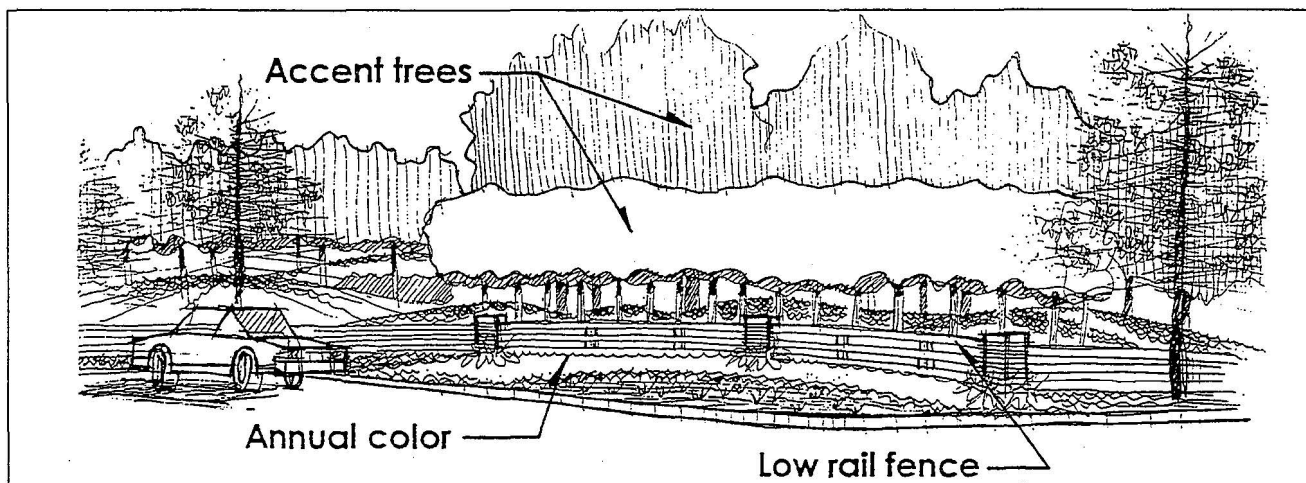
### A. Landscaping

1. Utilize informal landscape plantings which are harmonious with the natural setting. Emphasize use of indigenous plant materials. Treat creeks and swales as amenities by incorporating them into the plaza areas.
2. Special landscape features should be encouraged at project entries and in plaza areas. Examples include water features, sculptures, raised planters, signs, decorative paving, or other similar features.
3. Trees should be distributed evenly throughout the parking area so that 50% of the area should be shaded at tree maturity. Canopy trees should normally achieve a minimum diameter of 25 ft.. See plant list for appropriate parking area trees.

4. Motorist and pedestrian views of long expansive building walls, fencing or paving should be visually broken with appropriate landscaping. Blank walls adjacent to driveways, service areas and parking lots with no substantial landscaping should incorporate trellis work with vines to soften the overall appearance.



*Informal landscaping in the setback area.*



*Special landscape features at project entries.*

### 5.3 ARCHITECTURE

In the Country Commercial area, the architecture will be closely integrated with the landscape. The architectural style will reflect the Mediterranean (stucco, parapets), or agrarian flavor (barns, farmhouses). These developments will have an overall scale that is larger, with a greater emphasis towards vehicular orientation.

#### A. Front & Rear Facades

1. Tower elements used as architectural landmarks are encouraged. The height of vertical towers should relate positively to the main building. Towers should not exceed twice the height of the adjacent building elements.
2. Sculpt building mass and roof lines to highlight building entries, corners and plaza areas.
3. Encourage use of colonnades, porticoes, pergolas, relief of long facades through projecting or recessed elements, and use of first floor architectural treatment, which contribute to a positive human scale.
4. Single story buildings greater than 7,500 sq. ft. should be required to break or terrace monotonous roof lines. The building elevations should be stepped back on the sides and at the front of the buildings. Design features such as arbors, overhangs, large scale entry highlights and major landscape features should be used in combination with these large structures. These landscape elements will establish a comfortable human scale.
5. The side and rear elevations of the building should incorporate qualities and levels of detailing reflective of those on the main facade.
6. Continuous arcades along the front of commercial centers are encouraged. This feature provides pedestrian protection from the climatic elements, and allows movement from one shop to another without having to leave the arcade. Awnings, louvers, and trellises are also appropriate for this purpose.



*Tower element at building corner as a vertical landmark.*



*Sculpted building mass with towers.*



*Use architectural detailing to scale larger buildings.*

7. Franchised businesses should be encouraged to sensitively design their buildings with a character that is consistent with the predominant architectural styles.

#### **B. Entries & Window Displays**

1. Orient major building entries towards the street, as well as to the off-street parking areas.
2. Use elements such as towers, roof variations, awnings, colonnades, arches, covered porches or sculptures to highlight and identify entries from Hwy. 12.
3. Side walls of buildings adjacent to the pedestrian areas should incorporate interesting display windows and secondary entries.

#### **C. Awnings**

1. An awning should not appear as an add-on element, but rather should complement the architectural detailing and materials used in the building.
2. Where a storefront is extremely long, awnings should be broken into smaller modules related to window openings.

#### **D. Color & Material**

(Refer to 4.3 D, Page 39)



*Porticoes and colonnades contribute to the human scale.*



*Rear elevation to incorporate details reflective of the main facade.*



*Awnings in modules in relation to the windows and entries.*

## 5.4 SIGNAGE

### A. Attached Signs

In multi-tenant buildings, the sign program should include common frame design, background color and material for the sign face.

Roof signs or signs which extend above the parapet line of a facade should not be allowed with the exception of the theater marquee.

#### Wall Signs

1. Wall signs may include individual solid letters, individual internally illuminated letters, a board sign (i.e., individual letters displayed on a flat surface which is then mounted on the building face) or lettering painted directly onto the building face.
2. Boxed or cabinet display signage (i.e., a metal frame containing internal illumination and a facing material containing the lettering) is discouraged.
3. All wall signage should fit within a "Signable Area" on the wall surface uninterrupted by doors, windows or architectural details. The "Signable Area" should not exceed 20% of a building facade.
4. The individual letters or attached board sign should not exceed  $\frac{2}{3}$  of the height of the "Signable Area" or 18 inches in height.

#### Signage on Awnings

1. Signage can occur on both the awning canopy or the valance. Letter height on awnings or valances should not exceed 8 inches in height. The use of logos or symbols depicting the unique nature of a business is encouraged on the sloped face of awnings. The image area should not exceed 10% of the total sloped awning face.



*Sign wall at the recessed entry alcove.*



*Wall signs could be located between the awning and roof fascia.*



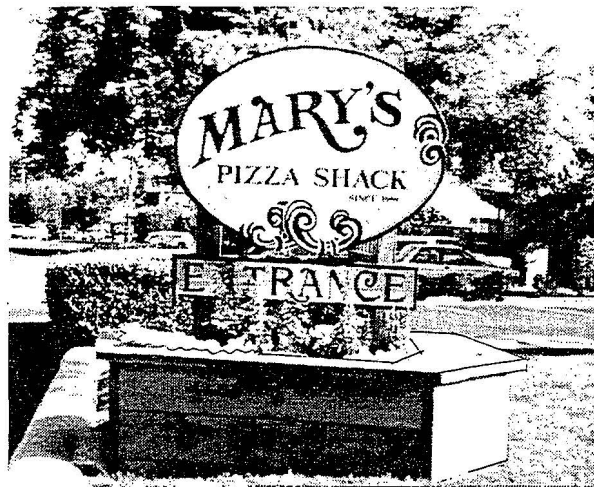
*Signage on awning valance.*



## B. Detached Signs

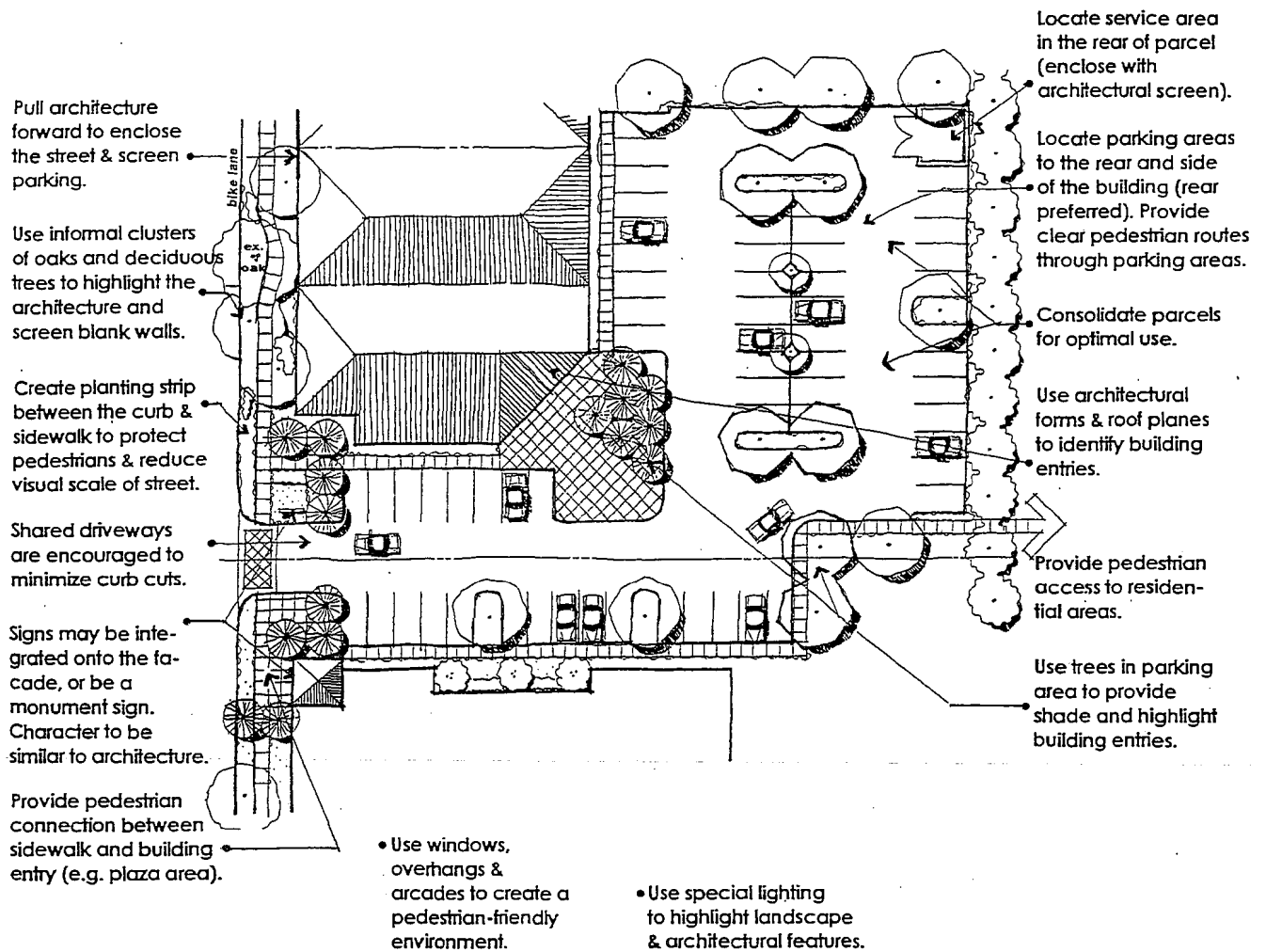
All detached signs should primarily be of monument-type design. The materials used in the sign frame and base should be constructed of the same materials used in the building, or be high quality, complementary materials. The following criteria should be met.

1. The sign should be integrated into the landscape design.
2. Maximum height should not exceed 6 ft..
3. Maximum area should not exceed 32 sq. ft..
4. Detached signs must conform to setbacks for architecture. Signs may be located in right-of-way if it does not conflict with sightlines or pedestrian circulation.
5. Signs should be located to avoid conflicts with sightlines of both autos and pedestrians.
6. Where multiple detached signs occur along the Highway, a minimum of 75' separation between signs is desirable.
7. Encourage creative monumental signage which reflects the special character of businesses or which integrates well with the landscape. Stone bases, special plantings or custom iron work is encouraged.
8. Banners and flagpoles are permitted where they are integrated into the overall landscape design.



*Creative images of detached signs.*





## COUNTRY COMMERCIAL

# ***CORRIDOR RESIDENTIAL***

Chapter 6

CORRIDOR RESIDENTIAL

corridor residential

*corridor residential*

## 6. Corridor Residential

The following guidelines aim to protect residential developments from the impact of Hwy. 12, and to create a positive relationship between commercial and residential uses.

### 6.1 SITE PLANNING

#### A. Right-of-Way Treatments

1. Perimeter privacy fence should be at a suggested setback of 17 ft. from the face of the curb. Buildings should be setback an additional distance from this edge as dictated by applicable zoning to side, front or rear yard setbacks.
2. Provide a meandering 5 ft. minimum concrete sidewalk separated from the curb.
3. Use informal clusters of evergreen and deciduous trees, shrubs, groundcover and gentle berms to soften the street edge and provide privacy for residences. Provide an average of one tree at 20 ft. on center in the setback area.
4. The streetlights should be Davit poles with Cobra head fixtures. Provide minimum level of light necessary for safety. Avoid an overlit appearance.



*A meandering sidewalk through informal landscape.*

#### B. Vehicular Circulation & Parking

1. Where parking areas abut Hwy. 12, provide a minimum 5 ft. wide landscape strip in addition to the right-of-way setback.
2. Minimize curb cuts on Hwy. 12 by consolidating driveways or accesses from side streets. Curb cuts on corner lots should be located at the farthest point away from the curb return as is feasible.
3. Screen views of parking areas from Hwy. 12. Screening may be a combination of low fencing, masonry wall or landscape materials.
4. The design of the carports should be similar to the architectural character of the development. Carport setbacks should be the same as those for the building.

#### C. Pedestrian Circulation

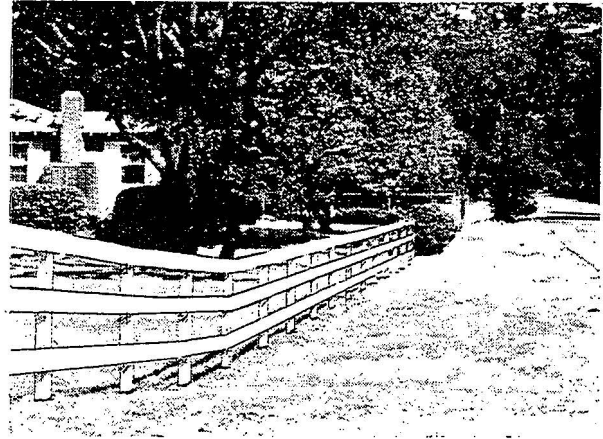
1. Provide pedestrian connections between residences or central common areas, and the sidewalks which is separate from the driveways. Develop pedestrian walkways on Hwy. 12 as a special feature through the use of trellises, low walls, ornamental fencing and focal plantings.



*Carports should complement the architecture style of the development. Vines on trellises provide screening.*

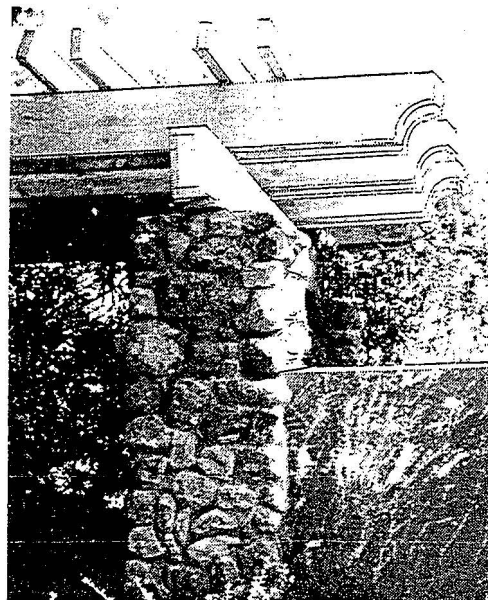
#### D. Service Areas

1. In multi-family developments, provide a central trash and utility area. Avoid locating trash enclosures where they can be seen from Hwy. 12 or major developments, pedestrian and auto circulation areas.
2. Trash enclosures should be a minimum 6 ft. high solid fence or wall which reflects the architectural character of the development.
3. Service areas visible from Hwy. 12 must be screened by a fence and landscaped.
4. The tops of trash receptacles which are visible from the second floor of a building should be screened with a roof or overhead trellis.
5. Trash enclosures should allow walk-in access without having to open the main enclosure gates.



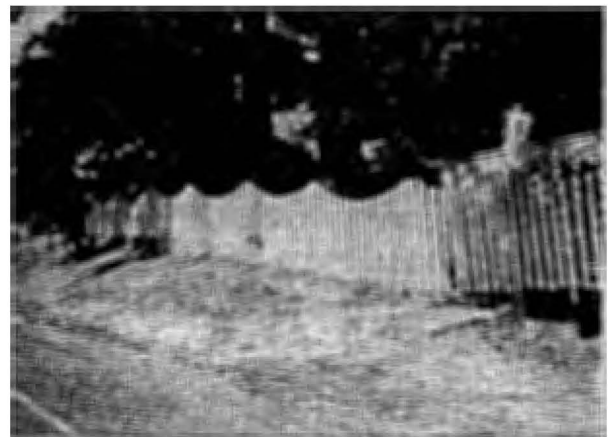
#### E. Fencing

1. A low wall or fence should be used to define the edge of the development. Where a privacy fence is required along Hwy. 12, a masonry or stucco wall should be used. Discourage wood fences along Hwy. 12 except for low ornamental fencing such as picket fences.
2. Avoid a monotonous tunnel effect along Hwy. 12 by varying the wall design or setback, and integrating the walls with the landscaping. Break continuous walls to allow views into common landscaped areas within the residential development.
3. Where the residential developments are adjacent to a creek or open space area, the use of a transparent fence such as a tubular steel fence is encouraged to allow views of these natural areas.



#### F. Lighting

1. Light fixtures should complement those used in the Main Street area. When possible, integrate bracket lighting mounted on the buildings for a residential ambience.



*Images of fencing and trellis element to punctuate stretches of perimeter wall.*

## 6.2 LANDSCAPE & SITE ELEMENTS

The informal, residential-scaled landscape within this area includes remnant orchard grids, rose trellises and grape arbors. Shade trees set in lawns are also appropriate images. Where creeks lie adjacent to the development, common outdoor areas will be located to optimize the use of these natural amenities.

### A. Landscaping

1. Landscape materials within the residential area should not be located such that they interfere with safe sight distances for vehicular, bicycle or pedestrian traffic; or where they conflict with overhead utility lines, or where they block sunlight from neighbors' windows or yards. Landscaping should be designed to provide winter sun and summer shade for residences.
2. Special landscape features should be encouraged at multi-family residential entries. Examples include water features, sculptures, raised planters, decorative paving, shade structures or a pool complex.
3. Where multi-family residential areas are adjacent to single-family residential areas, a six-ft. screening in the form of a wall or landscape planting may be required.



*Decorative paving and special planting at residential entries.*

## 6.3 ARCHITECTURE

### A. Front & Rear Facades

1. Building masses should be in character with the surrounding neighborhood. Break larger multi-family buildings into components in scale with existing single-family residences.
2. Articulate long lengths of building walls fronting Hwy. 12 with windows, wall projections and recesses, overhangs, chimneys, balconies, etc. Upper levels of the front facade should be stepped back as necessary to reduce perceived heights. Avoid presenting an inarticulated rear facade or continuous roof line to Hwy. 12.
3. Configure and orient buildings to create courtyards and usable open space areas that can be seen from Hwy. 12. Create a visual connection and a friendly pedestrian environment between residential areas and Hwy. 12.
4. The architecture should have a "finished" appearance. Avoid the use of exposed exterior stairs and tacked-on utility cabinets that are visible from Hwy. 12.
5. Detached garages and carports should be designed to blend with the architectural character.
6. The use of architectural details such as covered porches, entry stoops and clapboard siding which reflect the country character are encouraged.
7. In single-family developments, encourage use of car courts to minimize curb cuts for driveways. Introduce landscaping in courts, especially where visible from the street.

### B. Color & Material

(Refer to 4.3 D Page 39)

## 6.4 SIGNAGE

1. Integrate project signage onto the entry wall and landscape area. Freestanding signs are discouraged.
2. Entry wall signs may consist of individual solid letters, or flat signs which may be mounted, recessed, or painted on the wall.
3. The entry sign projects an image of the residential community to the public. The sign character should reflect architectural theme of the development. Therefore, intricate designs with artistic expressions (semi-relief murals, logos, etc.) are encouraged.



*Project signage integrated with the entry wall.*

Encourage use of  
rolled gutter & curb.

Provide vine-covered  
masonry wall or rural  
fence. Notch wall to  
create visual relief.

Use informal evergreen  
oaks and deciduous  
accent trees to buffer  
residential uses.

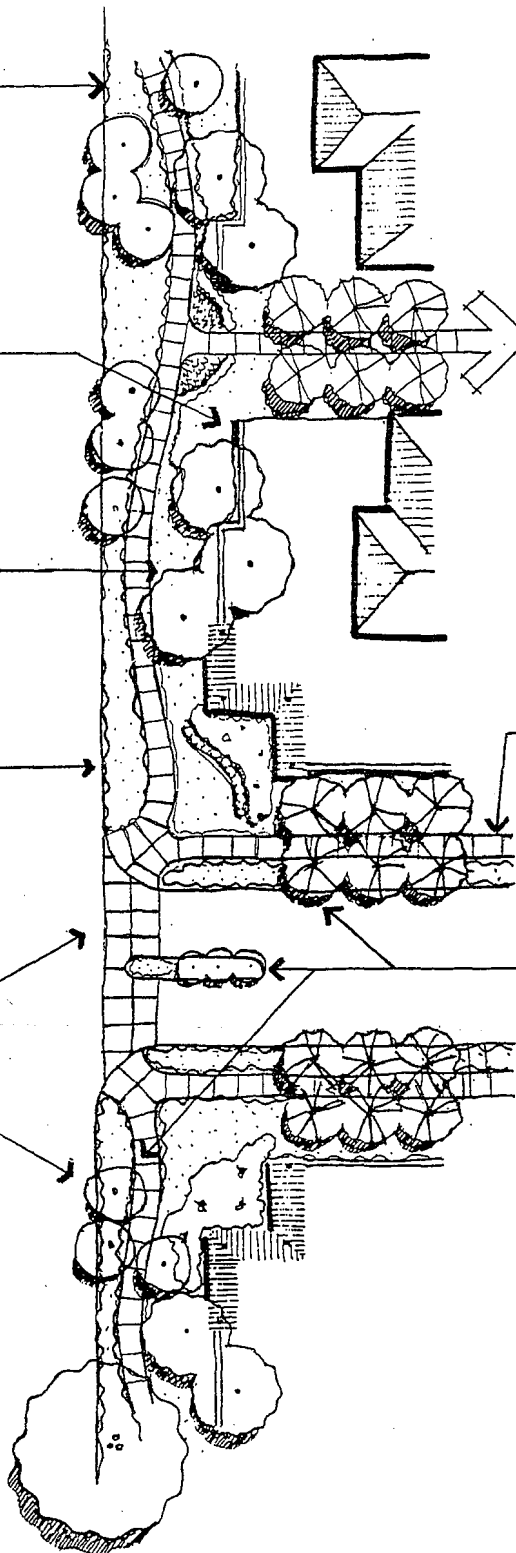
Separate meandering  
sidewalk from curb.

Create a clear entry  
statement. Pulled-  
back walls, trellises,  
monument features,  
special landscaped  
medians, special  
paving and planting  
can be used to  
highlight entries.

Avoid 'walled city'  
appearance. Pro-  
vide visual open-  
ings into the neigh-  
borhoods, & pro-  
mote pedestrian  
access.

Provide a pedestrian/  
bike link to sidewalk  
from project.

Blend project trees w/  
streetscape plantings.



## CORRIDOR RESIDENTIAL



# ***SITE ELEMENTS APPENDIX***

Chapter 7

SITE ELEMENTS APPENDIX

site elements appendix

*site elements appendix*

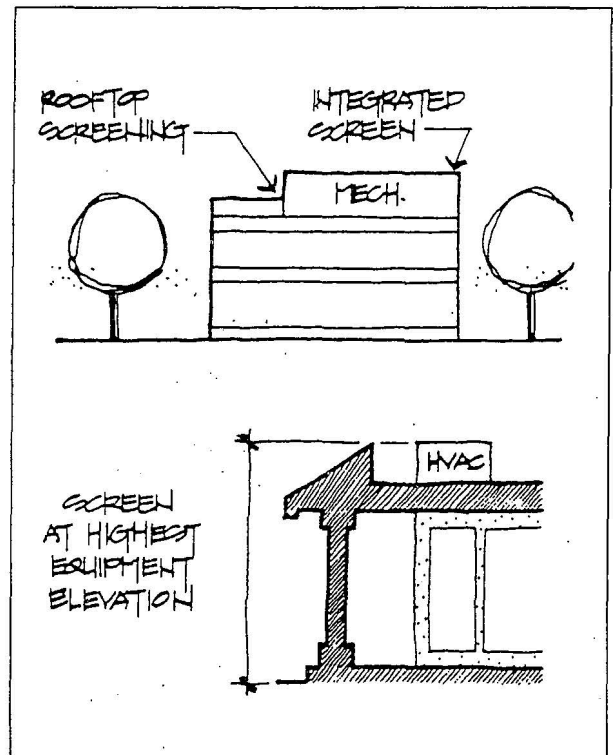
## 7. Site Elements Appendix

The Site Element Appendix consists of guidelines as well as product and plant material information. All guidelines apply to the 3 character areas with the exception of 'Street Furnishings', 'Awnings', 'Attached/Detached & Directional Signage' which only apply to the Main Street and Country Commercial areas.

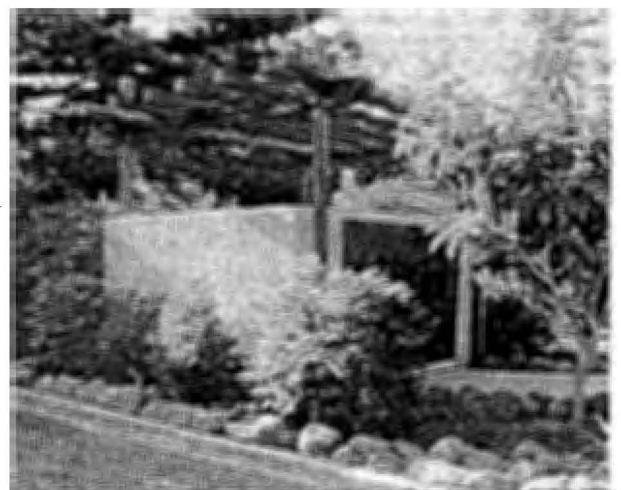
The appendix also illustrates and specifies the site elements used in the guidelines. The planting information aims to provide an appropriate palette for the Corridor.

### Service Areas

1. Screening of mechanical/electrical areas must take into account the noise level and fumes generated by such equipment. Use materials with noise attenuation properties and design screens that deflect fumes favorably away from the main part of the building as well as adjacent parcels.
2. Trash enclosures should be adequately sized to accommodate all refuse containers, including recycling bins. They should allow walk-in access without having to open the main enclosure gates.
3. If the trash enclosure is located adjacent to parking spaces or drives, it should be protected by a concrete-curbed landscape buffer strip a minimum of 3 ft. wide.
4. Service areas should be located where trucks being loaded or unloaded do not disrupt the flow of traffic within the project. Where possible, storage spaces should be integrated into this area.



Examples of equipment screening.

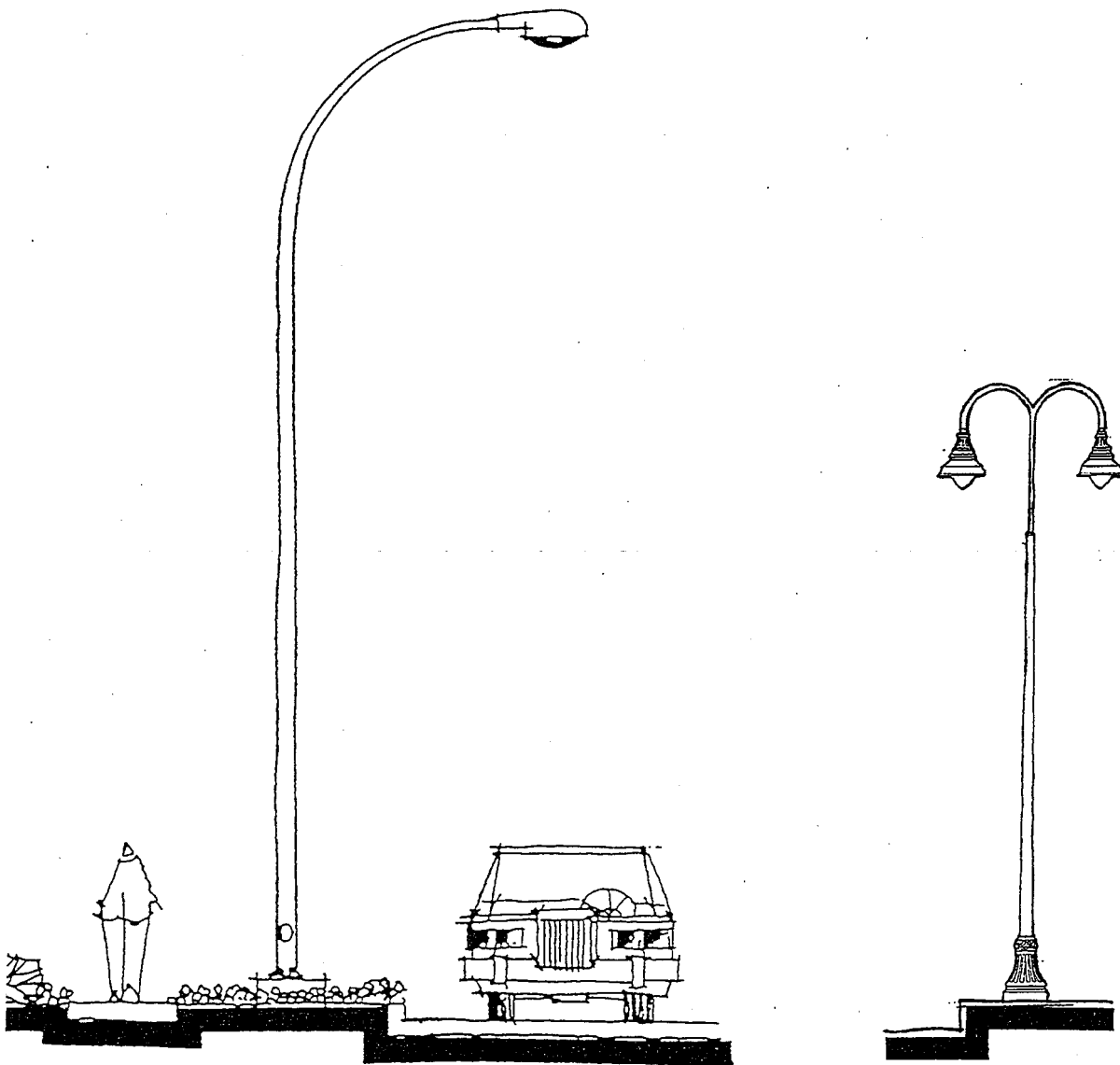


Trash enclosure with hinges and doors of adequate gauge to withstand heavy use.

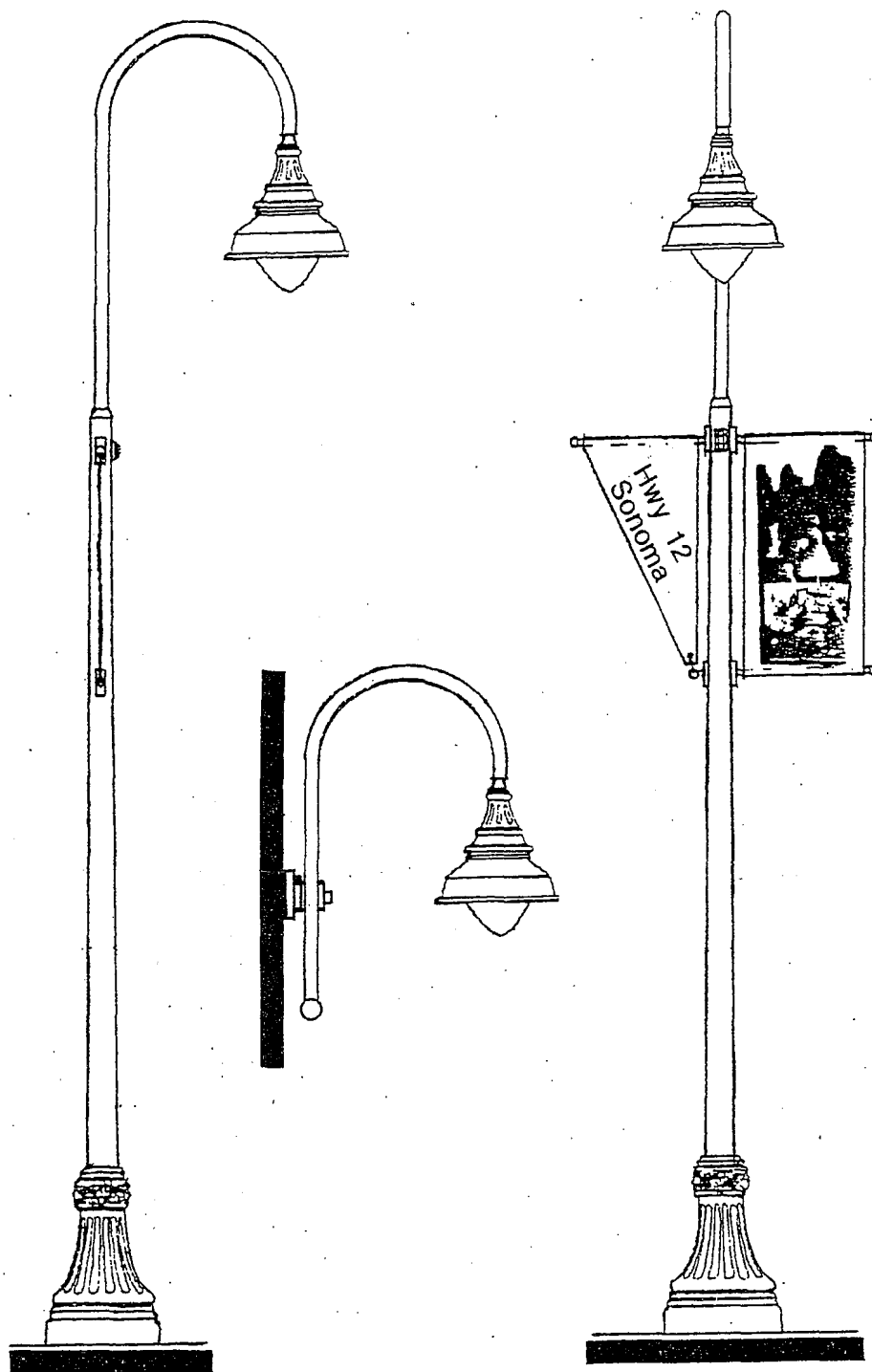
## Lighting

1. Streets should be adequately illuminated for safety and area highlights. Vehicular circulation lighting should consist of the Cobra head fixture mounted on Davit light poles, dark green in color to blend

with the natural background, per Caltrans specifications. Pedestrian areas should be illuminated by the Sternberg #1910 traditional single or twin lights on posts. Fixtures with banners could be included to add decorative color and liveliness, especially at pedestrian nodes.



*Street lighting at the Country Commercial and Corridor Residential areas (left) and at the Main Street (right).*

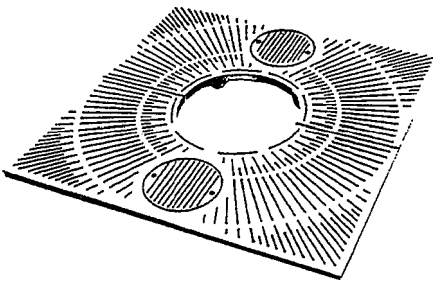


*Lighting at the Main Street pedestrian areas include building-mounted lanterns (left). Banners (right) should be incorporated onto the lighting fixture for festive occasions. Verde green colored fixtures to blend with the background.*

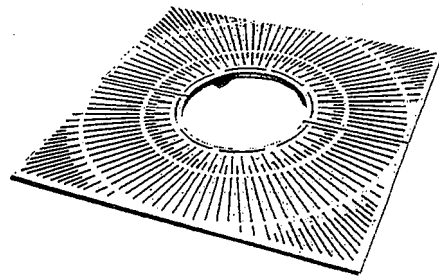
## Street Furnishings

A uniform street furniture program should be developed to provide attractive, high quality benches, trash receptacles, bike racks and paving highlights in appropriate areas. Retain a design consultant to prepare improvement plans for the streetscape design.

1. Street furniture such as pots, trash containers, benches, etc., should be clustered whenever possible.
2. Planters should be used to add rhythm to the streetscape and pedestrian routes. A variety of planter sizes and heights should be clustered for an interesting composition. The Florence series from Magnalite is recommended in SB85 sand-blast finish. Ash urns of the same series should be used.
3. Tree grates should be used at pedestrian nodes and plazas for a higher quality setting. Grates should be of a natural patina finish and should meet all handicapped requirements. Recommended are Neenah cast iron grates, R-8706-1 180 degrees square, and R-8706 180 degrees square with light cutouts.
4. Benches should be located at pedestrian nodes and plazas. The length of these benches, or a middle arm rest should be taken into consideration to avoid undesirable usage. Recommended benches are Timberform's straight or arched backs in grey green or white.
5. Bike racks should be strategically located, and be of a color that complements the furniture palette. Shown is the Cyclops 2170-11 from Columbia Cascade Company.



For use with sub-grade lighting, with light opening grates bolted down. Light opening grate is 10 $\frac{1}{2}$ " diameter for 9" opening.  
Note  $\frac{1}{4}$ " slot openings for special pedestrian requirements. Two pieces per set with expandable tree opening. Available with cast iron angle frame, if required.

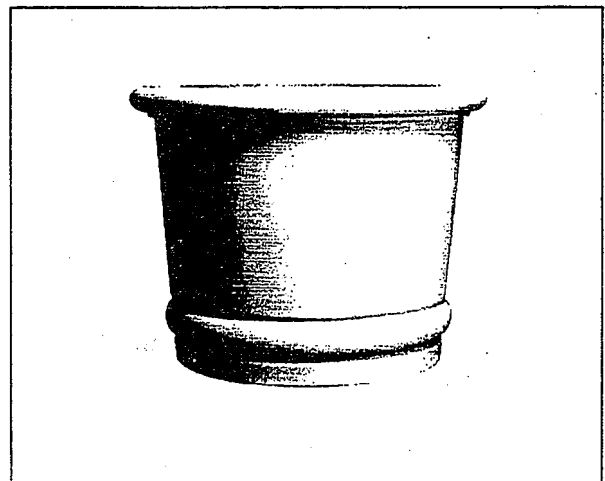


Note  $\frac{1}{4}$ " slot openings for special pedestrian requirements. Two pieces per set with expandable tree opening. Available with cast iron angle frame, if required.

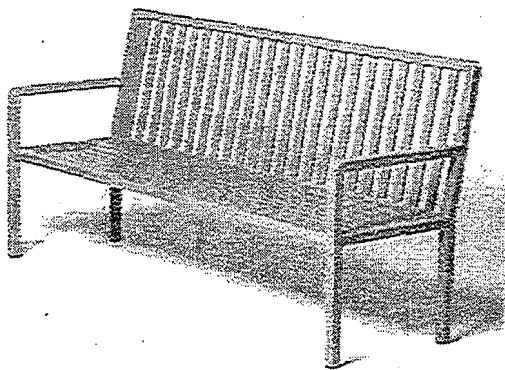
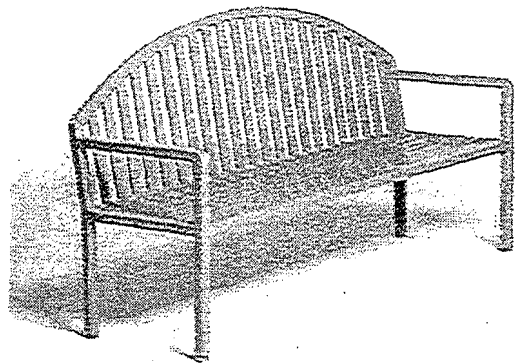
*Use tree grates for a higher quality setting. Available with cutouts for uplights (left).*



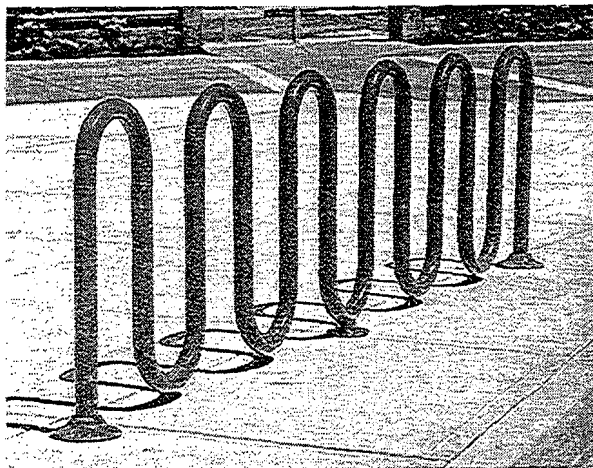
*Tree grates could be coordinated with the paving pattern for creative solutions.*



*Use high quality planters to add rhythm to the streetscape.*



*Benches with arched or straight backs, in grey green or white.*



*Bike racks should be included in the streetscape palette.*

## Awnings

1. Straight awnings with 12" valances are recommended. Supporting arms at the sides of canopies should be painted if exposed and connected to the building to provide extra strength during windy periods of the year. Canopy structures should be a minimum of 8 ft.-6" above the sidewalk. Valances should be a minimum of 7 ft.-6" above the sidewalk.
2. Awnings should be fabricated from cloth, not wood or aluminum. The most suitable material is 100% solution dyed acrylic such as Sunbrella and Dickson 32 fabrics. All canopies should be fire retardant and guaranteed against fading. Awnings should be or solid colors, not prints or stripes.
3. Awning colors should generally be darker than the building base color. Bright or electric awning colors are not desired and translucent awning materials with internal lighting (i.e., under the awning) should not be allowed. Awning colors which harmonize with the street furniture's deep green colors, and the brick and tile materials on the existing buildings are the most appropriate. Use colors such as deep burgundies, greens, teals and tans. Avoid bright oranges, reds, yellows or chartreuse.

## Signs (General)

1. The scale of signs, letters, and symbols should be appropriate to their use, whether to attract the attention of a passing motorist or a strolling window shopper. Discourage multiple signs on a single business which are repetitive in nature, and confusing signs which are difficult to read and/or too small in scale for auto traffic. Limit temporary signs indicating sales and promotions, in particular large temporary signs which are used to attract motorists.
2. A merchant may place the sign (indicating hours of business, emergency telephone numbers, street address, etc.,) on the storefront or in the display showcase. The sign may be gold or silver leaf etched, sandblasted or cast resin lettering, not exceeding 2" in height nor more than 144 sq. inches.

3. Signage should occur on both facades of corner buildings which face the street when it does not negatively impact the adjacent residence.
4. All buildings or businesses should have a street number displayed in a location which can be clearly seen by passing motorists.
5. No exposed lamps or tubing will be permitted with the exception of well-designed neon signs or where they are a part of the historical character of the sign.
6. All signs should bear the U.L. label, and their installation should comply with all local building and electrical codes. No exposed raceways, cross-overs or conduits will be permitted. All cabinets, conductors, transformers or other equipment will be concealed from public areas.
7. Colors should be used appropriately. Limited use of several colors with strong contrast between background, text and/or logo is recommended to make the signs easy to read. Too many colors reduce legibility and should be avoided.
8. Signs should be constructed with quality materials and in a craftsman-like manner to ensure both an attractive appearance and a durable product. All signs should be professionally designed and fabricated. Specially designed logos, symbols and icons are encouraged.

#### **Directional Signs**

1. Signs should be low key and located to avoid conflicts with sightlines of both autos and pedestrians.
2. Maximum height should not exceed 4 ft.
3. Maximum area should not exceed 16 sq. ft.

#### **Plant Materials**

The following are general guidelines only. The final selection of plant materials should be made after site-specific soil tests have been carried out. Careful consideration should also be given to the functional and horticultural constraints of the plant type.

In evaluating street trees, shrubs and groundcover, the following guidelines were established:

1. The plant must be horticulturally compatible and visually appropriate to the Hwy. 12 corridor.
2. The plant must be consistent with the streetscape criteria in drought tolerance, maintenance requirements and availability.
3. The plant must be adaptable to the urban environment.
4. Formal plantings are appropriate in pedestrian areas such as building entries or plaza areas, and also in narrow spaces such as parking median or planting strip.
5. Informal plantings are appropriate in natural settings along the country road.
6. For a formal shrub appearance, plant evenly spaced rows or grids of a single species. For an informal appearance, vary the shrubs spacing and cluster with another species. Undulate the shrub line to relate to topography. Provide periodic breaks in the shrub cluster.
7. Plant in masses of single species. Space shrubs to grow together in 2-3 years.
8. Prune appropriately to encourage individual plants to grow together into a mass. Avoid pruning which create a line of individual "balls". Prune formal plantings in a more formal manner (e.g., shearing) and prune informal planting to allow the natural form to develop.
9. Groundcover should be spaced to allow for complete infill within 9 months of planting.



- 
10. Landscape materials should not be located such that they interfere with safe sight distances for vehicular, bicycle or pedestrian traffic; or where they conflict with overhead utility lines; hinder pedestrian or bicycle circulation. Plants which drop seed pods or fruit should not be located where they would safety problems along the sidewalks.
  11. All planting should be maintained in good condition. Therefore, maintenance requirements for various plant types should be taken into consideration when determining the appropriateness of the plant material. Such maintenance should include, where appropriate, pruning, mowing, weeding, cleaning of debris and trash, fertilizing and regular irrigation.
  12. Landscape materials should be located to reduce the use of energy for cooling. Use of deciduous plants for landscaping should encourage summer shade on walls, windows, roofs, parking spaces, drives and walks to improve energy efficiency.
  13. During construction, protective barriers should be erected around the drip line of all existing trees to be preserved, and drainage patterns around existing trees should remain undisturbed for their preservation.
  14. Irrigation sprinkler layout should be efficient and designed to minimize the amount of spray that will fall on sidewalks, neighboring properties, and adjacent buildings. Automatically controlled irrigation systems should be fully maintained in sound operating condition with sprinkler and drip heads periodically cleaned or replaced, to ensure continued regular watering of all landscaped areas.

### **Plant Matrices**

The matrices in the following pages provide a palette of plant material which is appropriate for the Corridor. The palette is by no means all inclusive, and variations may be allowed where appropriate.

1. The tree matrix is categorized into large/medium stature trees, accent species and palms. The choice between a large or medium stature tree depends on the spatial limitation of the location.
2. The shrub/vine/groundcover matrices allow for personalization through varied combinations of plant type. Property owners should work with the architecture when choosing an array of plant material.

## Trees

Botanical Name	City of Sonoma Street Tree List	Common name	Main Street Trees	Comm"/Res" St. Trees	Parking Lot	Creek or Swale	Screen Tree	Remarks
<b>Large Stature Trees</b>								
1. Celtis sinensis		Chinese Hackberry		•	•			Resist. to Dutch Elm disease
2. Fraxinus oxycarpa 'Raywood'		Raywood Ash	•					Special pruning required.
3. Liriodendron tulipifera	•	Tulip Tree				•		Very large tree
4. Magnolia grandiflora 'Russet'	•	Russet Southern Magnolia					•	
5. Platanus acerifolia 'Yarwood'	•	Yarwood London Plane Tree	•			•		Anthraxnose/mildew resistant variety
6. Platanus racemosa		Sycamore				•		
7. Quercus agrifolia	•	California Live Oak		•		•		Drought tolerant
8. Quercus coccinea	•	Scarlet Oak		•				
9. Quercus ilex	•	Holly Oak		•				
10. Quercus palustris	•	Pin Oak		•				
11. Quercus lobata		Valley Oak		•				
12. Quercus suber		Cork Oak		•				
13. Quercus virginiana	•	Southern Live Oak		•				
14. Schinus molle		California Pepper		•			•	
15. Sequoia sempervirens		Coast Redwood				•		
16. Umbellularia californica		Bay Oak				•		
<b>Medium Stature Trees</b>								
1. Acer rubrum 'October Glory'	•	Red Maple		•				Needs watering
2. Aesculus californica		Buckeye				•		
3. Aesculus carnea	•	Red Horsechestnut		•				
4. Laurus 'Saratoga'	•	Saratoga Laurel					•	
5. Maytenus boaria 'Green Showers'	•	Green Showers Mayten Tree				•		
6. Pistacia chinensis	•	Chinese Pistache		•	•			Fall color
7. Podocarpus gracilior	•	Fern Pine					•	
8. Pyrus calleryana	•	Flowering Pear	•					Disease-resistant cultivar
9. Robinia idahoensis		Idaho Locust		•			•	Not near paving
10. Sapium sebiferum	•	Chinese Tallow Tree		•				Small. Great Fall color.
11. Tilia cordata 'Greenspire'	•	Littleleaf Linden		•	•			Fruit litter

Botanical Name	City of Sonoma Street Tree List	Common name	Main Street Trees	Comm'l/Res'l St. Trees	Parking Lot	Creek or Swale	Screen Tree	Remarks
<b>Accent Trees</b>								
1. <i>Acer palmatum</i>		Japanese Maple		•				
2. <i>Arbutus unedo</i>	•	Strawberry Tree	•			•	•	
3. <i>Cercis canadensis</i>	•	Eastern Redbud		•		•		Needs good drainage
4. <i>Crataegus phaenopyrum</i>	•	Washington Thorn	•					Thorns
5. <i>Lagerstroemia indica</i> X <i>Fauriei</i>	•	Crape Myrtle Hybrids	•					Use mildew resistant hybrids. August color
6. <i>Ligustrum lucidum</i>	•	Pivet Tree					•	
7. <i>Magnolia grandiflora</i> 'St. Mary'		St. Mary Southern Magnolia	•					
8. <i>Malus floribunda</i>	•	Flowering Crabapple	•					Select fireblight - resistant varieties
9. <i>Nerium oleander</i> std	•	Oleander	•				•	Avoid windy locations
10. <i>Photinia fraseri</i> std.	•	Photinia					•	
<b>Palms</b>								
1. <i>Phoenix canariensis</i>	•	Canary Island Palm	•	•				Use palms for accent and highlight
2. <i>Washington filifera</i>	•	California Fan Palm	•	•				
3. <i>Washington robusta</i>	•	Mexican Fan Palm	•	•				
4. <i>Chamaerops humilis</i>	•	Mediterranean Fan Palm	•	•				
5. <i>Erythea armata</i>	•	Mexican Blue Fan Palm	•	•				
6. <i>Erythea edulis</i>	•	Guadalupe Fan Palm	•	•				
7. <i>Trachycarpus fortunei</i>	•	Windmill Palm	•	•				

## Shrubs

Botanical Name	Common Name	Small Shrub	Medium Shrub	Large Shrub	Drought Tolerant	Remarks
<b>Shrubs</b>						
1. <i>Acacia redolens</i>	Acacia	●			●	2-2½' tall, 25' spread
2. <i>Agapanthus africanus</i>	Lily of the Nile		●		●	
3. <i>Arbutus unedo</i> 'Compacta'	Strawberry Tree		●	●	●	Dependable
4. <i>Arctostaphylos densifolia</i> 'Howard McMinn'	McMinn Manzanita			●	●	
5. <i>Arctostaphylos manzanita</i>	Manzanita			●	●	
6. <i>Baccharis pilularis</i> 'Twin Peaks'	Coyote Brush		●		●	
7. <i>Buxus microphylla japonica</i>	Japanese Boxwood		●		●	
8. <i>Cheiranthus cheiri</i> 'Bowles Mauve'	Wall Flower				●	
9. <i>Cistus hybridus</i>	White Rockrose		●		●	
10. <i>Cistus purpureus</i>	Orchid Rockrose		●		●	
11. <i>Cistus salviifolius</i> 'Prostratus'	Sageleaf Rockrose	●			●	
12. <i>Cistus skanbergii</i> 'Low Pink'	Pink Rockrose	●			●	
13. <i>Coprosma kirkii</i>	Coprosma	●			●	
14. <i>Cornus stolonifera</i>	Red Twig Dogwood			●		Creekside, shade
15. <i>Correa alba</i>	Australian Fuchsia		●		●	Needs fast drainage
16. <i>Correa pulchella</i> 'Carmine Bells'	Australian Fuchsia		●		●	Needs fast drainage
17. <i>Diosma pulchrum</i>	Pink Breath of Heaven		●		●	Needs fast drainage
18. <i>Eriogonum speciosum</i>	Buckwheat		●		●	Sun, slopes
19. <i>Escallonia 'Fradesii'</i>	Escallonia			●	●	Attracts bees
20. <i>Escallonia 'Terri'</i>	Escallonia		●		●	Attracts bees
21. <i>Euryops pectinatus</i>	Euryops		●			Needs good drainage
22. <i>Felicia amelloides</i>	Blue Marguerite	●				
23. <i>Grevillea 'Noellii'</i>	NCN		●			Needles resist deers
24. <i>Hemerocallis species</i>	Daylilies		●			Water thoroughly while blooming
25. <i>Heteromeles arbutifolia</i>	Toyon			●	●	
26. <i>Iris douglasiana</i>	Douglas Iris		●		●	
27. <i>Ligustrum japonicum</i> 'Texanum'	Texas Privet			●		Good hedge
28. <i>Liriope muscari</i>	Big Blue Lily Turf	●				Small, shade
29. <i>Myoporum parvifolium</i> 'Putah Creek'	Myoporum			●		Needs good drainage
30. <i>Myrtus communis</i> 'Compacta'	Dwarf Myrtle		●			
31. <i>Nandina domestica</i>	Heavenly Bamboo		●			Tall, good for narrow strips
32. <i>Nerium oleander</i>	Oleander		●		●	
33. <i>Oenothera berlandieri</i>	Mexican Evening Primrose	●			●	Invasive. Good on slopes

## Shrubs, Vines & Groundcover

Botanical Name	Common Name	Small Shrub	Medium Shrub	Large Shrub	Drought Tolerant	Remarks
<b>Shrubs</b>						
34. Pittosporum tobira	Tobira			•		
35. Pittosporum tobira 'Wheeler's Dwarf'	Dwarf Tobira	•				
36. Rhamnus californica 'Ballerina'	India Hawthorn		•		•	
37. Ribes sanguineum	Pink Winter Current		•			Part shade
38. Ribes viburnifolium	Evergreen Current	•			•	Excellent under native oaks
39. Sollya heterophylla	Australian Bluebell Creeper		•			Shade
40. Spiraea species	Spiraea		•			
41. Rhamnus californica 'Eve Case'	Coffeeferry			•	•	
42. Tulbaghia violacea	Society Garlic	•				Small, hardy
<b>Vines</b>						
1. Clytostoma callistegioides	Violet Trumpet Vine				•	Needs support
2. Gelsemium sempervirens	Carolina Jasmine				•	Needs support
3. Hardenbergia violacea	Lilac Vine				•	
4. Jasminum polyanthum	NCN				•	Needs shaded roots
5. Lonicera japonica	Japanese Honeysuckle					Needs strong support
6. Parthenocissus tricuspidate	Boston Ivy				•	
7. Rosa Climbing species	Roses				•	
8. Solanum jasminoides	Potato Vine				•	Deer resistant
9. Vitis species	Grape Vine				•	
10. Wisteria sinensis	Wisteria				•	
<b>Groundcover</b>						
1. Arctostaphylos species	Manzanita				•	
2. Ceanothus species	California Lilac				•	May be shortlived
3. Cotoneaster 'Lowfast'	Cotoneaster					
4. Fragaria chiloensis	Wild Strawberry					Part shade
5. Gazania mitsuwa species	Clumping Gazania-Mitsuwa				•	White / orange / red
6. Rosmarinus officinalis 'Prostratus'	Creeping Rosemary					
7. Trachelospermum asiaticum	Asiatic Jasmine					
8. Trachelospermum jasminoides	Star Jasmine					
9. Vinca major / minor	Periwinkle				•	Shade

## ***8. Written Public Comments Appendix***\_\_\_\_\_

The following are written public comments recieved on the Design Guidelines.

# Sonoma Valley Citizens Advisory Commission



Sonoma, California

September 6, 1994

Board of Supervisors  
County of Sonoma  
Santa Rosa, California.

Re: Sonoma Highway 12 Design Guidelines.

Gentlemen:

The Sonoma Valley Citizens Advisory Commission at its regular meeting of August 24, 1994, received presentations of the draft Highway 12 Design Guidelines by the Redevelopment Agency and the consultant, David Gates & Associates. Public comments were in strong favor of the basic stance of the report which sets forth effective steps to energize redevelopment of the Valley communities. The Valley should develop its own identity and its constituent communities should also develop their own within a larger Valley-wide image. The report shows how this goal can be attained through design improvements along Highway 12 as the spine of the Valley.

After deliberation, the Commission unanimously carried a motion to recommend the County adopt the Guidelines with the following comments:

1. County General Plan should be amended to reflect the public desire to keep Highway 12 through the Valley to three traffic lanes.

The Overwhelming majority of public opinion is for three traffic lanes, and not five, for the Highway 12 throughout the valley. The report is written for three traffic lanes but without precluding the possibility of five lanes. There have been strong public opinions not to leave the matter ambivalent. This issue will surface continually as long as the County General Plan designates 5-lane right-of-way for Highway 12. Clarification of policy is needed here.

2. We recommend demonstration project(s) of public facility improvement as a joint effort of public agencies.

In order to materialize the design improvements, the most effective way is to implement first the public facility improvements throughout the corridor. Mr. Bane of the Redevelopment Agency stated that there would not be sufficient public funding to do it all at once, but select pilot project(s) could be undertaken in cooperation with Caltrans and County. We urge that such cooperative undertaking be made as soon as possible to demonstrate an actual example and keep the momentum strong.

-Continued.



Board of Supervisors,  
September 6, 1994,  
Page Two.

3. The report should include a design guide to the corner of West Napa Street and Sonoma Highway.

Design transition between the City of Sonoma and the corridor will take place on the Sonoma Highway between West Napa Street and the Southern Gateway. In order to ensure design harmony between the two, recommendations should be made to the City for the design treatment of this transition area. This issue could be addressed in the design guidelines that the City is contemplating in the current General Plan update.


4. Consultant should clarify the importance of "goals" and the illustrative nature of "guidelines" and "recommendations," and explore better mechanisms of design review and guideline update.


In the draft report the "vision" and "goals" are beautifully formulated. "Recommendations" and "design guidelines" are specific enough for the Redevelopment Agency, County Design Review Board and Planning Department to administer them from Santa Rosa. However, in order to encourage indigenous design improvements and nurture unique community development, the guidelines should leave some flexibility. Local public input should be maximized for design preparation, review and future update of guidelines. Consultant should present innovative mechanisms for administering the Design Guidelines so that the vision and goals of the Valley may one day become a reality.

5. Sonoma Valley Chamber of Commerce be encouraged to continue its efforts to further the process with the support of this Commission.

Sonoma Valley Chamber of Commerce through its Valley Improvement Program has made commendable efforts to bring this project to fruition. We request the Chamber to continue its efforts to further the progress with the support of this Commission.

Respectfully submitted,

  
Robert Gemmell, Co-Chair

  
Pamela Stephens, Co-Chair

cc: Mr. Tom Bane, Redevelopment Agency  
Mr. David Gates, David Gates & Assoc.  
Mr. Ignazio Vella, City of Sonoma  
Mr. Hal Beck, Chamber of Commerce

# City of Sonoma

## Administration

No. 1 The Plaza  
Sonoma, California 95476-9000



## Sonoma Sister Cities:

Chambolle-Musigny, France  
Greve in Chianti, Italy  
Kaniv, Ukraine

November 3, 1994

Sonoma Valley Citizens Advisory Commission  
Robert Gemmell, Co-Chair  
969 Moon Mountain  
Sonoma, CA 95476

Dear Mr. Gemmell:

Enclosed please find a certified copy of City of Sonoma Resolution #79-94 supporting the design guidelines for Highway 12 between Verano Avenue and Agua Caliente Blvd. and the recommendations of the Sonoma Valley Citizens Advisory Commission.

Very Truly Yours,



Mary Neilan  
Deputy City Clerk

Encl.

cc: Sonoma Valley Chamber of Commerce  
Tom Bane, Sonoma County Redevelopment Agency  
Ignazio Vella  
David Gates, David Gates and Assoc.  
Supervisor Mike Cale, Sonoma County 1st District

THE WITHIN INSTRUMENT IS A TRUE  
AND CORRECT COPY OF THE ORIGINAL  
ON FILE IN MY OFFICE.

ATTEST.

CITY CLERK

CITY OF SONOMA

RESOLUTION 79-94

SUPPORTING THE DESIGN GUIDELINES FOR HIGHWAY 12 BETWEEN  
VERANO AVENUE AND AGUA CALIENTE BLVD. AND THE  
RECOMMENDATIONS OF THE SONOMA VALLEY CITIZENS ADVISORY  
COMMISSION

WHEREAS, residents, business owners and the Sonoma Valley Citizens Advisory Commission have been participating with the Sonoma County Redevelopment Agency in the preparation of design guidelines for Highway 12 through the Boyes Hot Springs, Feters Hot Springs, El Verano and Aqua Caliente areas of the Sonoma Valley; and,

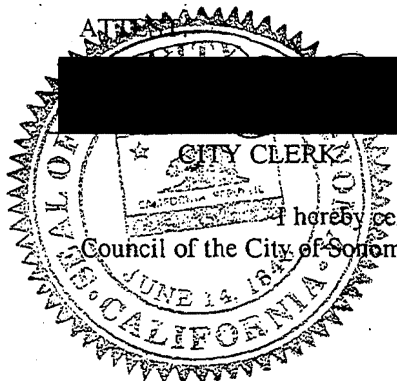
WHEREAS, the August 1994 draft of the Design Guidelines represent the culmination of months of work and input from members of those communities; and,

WHEREAS, the Sonoma Valley Citizens Advisory Commission, at its meeting of August 24, 1994, formally and unanimously adopted a motion to recommend to the Sonoma County Board of Supervisors that the guidelines be adopted.

NOW, THEREFORE, BE IT RESOLVED that the Sonoma City Council hereby concurs with and supports the recommendation of the Sonoma Valley Citizens Advisory Commission to the Sonoma County Board of Supervisors to adopt the Highway 12 Design Guidelines, and further supports the recommendations of the Commission contained in their letter to the Board of Supervisors dated September 6, 1994, and attached hereto as Exhibit "A".

The foregoing Resolution was duly adopted this 5th day of October, 1994, by the following roll call vote:

AYES: (4) Clm. Carter, Ramponi, Riboni, Mayor Cernak  
NOES: (0) None  
ABSENT: (1) Clm. Cannard  
ABSTAINING: (0) None



I hereby certify that the foregoing resolution was duly and regularly passed by the City Council of the City of Sonoma at a regular meeting thereon held October 5, 1994.

CITY CLERK

MACNAIR  
&  
ASSOCIATES  
CONSULTING HORTICULTURISTS



September 1, 1994

Mr. Tom Bane  
Redevelopment Manager  
Sonoma County Community Development Commission  
1440 Guerneville Road  
Santa Rosa, CA 95403-4107

RE: Draft Report Highway 12 Design Guidelines

Dear Mr. Bane:

As a past resident of the Boyes Springs and El Verano areas, I found the report prepared by Gates and Associates well written and thoughtful. The recommendations are appropriate and, when implemented, will significantly improve the area.

Following are comments pertaining to the plant selection lists in the Site Elements Appendix. These are technical concerns pertaining to horticultural and arboricultural issues.

- 1.) Many of the large and medium stature trees have surface root systems which are capable of curb and pavement damage. The design criteria should include minimum required planting areas to sustain the trees, as well as the installation of linear or panel type root barriers installed parallel with concrete structures.
- 2.) The suggested tree selections for Main Street and Parking Lots are too limited and, in some cases, inappropriate. There are only three tree selections for the Main Street Areas. For Parking Lots, *Liriodendron tulipifera* attracts heavy aphid infestation with exudate drip, and *Acer rubrum* is not tolerant of parking lot conditions (high temperatures due to reflected heat).

There are no selections for the Corridor Residential tree plantings.

I suggest there be a single list segregated by stature, with minimum required planting areas listed. The landscape architect can then make the appropriate selection based upon actual site conditions with more flexibility.

- 2.) *Fraxinus uhdei* should not be on the list due to its poor structural characteristics and aggressive surface roots.
- 3.) *Fraxinus oxy.* 'Raywood' requires special pruning to develop into an acceptable street tree. It would be beneficial if both structural and root quality specifications for all trees were included in the guidelines.

Draft Report Highway 12 Design Guidelines  
Page 2  
September 1, 1994

- 4.) *Platanus acerifolia* 'Bloodgood' is very susceptible to mildew in the Sonoma area. *Platanus racemosa* is very susceptible to anthracnose and is not native to the area.
- 5.) *Quercus robur* is susceptible to serious mildew infection.
- 6.) The bark of *Quercus suber* is very soft and susceptible to vandalism.
- 7.) The variety of *Acer rubrum* should be specified as 'October Glory'. This selection is the most dependable for fall color in milder climates.
- 8.) *Pyrus calleryana* is no longer recommended as a primary street tree due to high incidence of the bacterial disease *Pseudomonas syringae*. All varieties are assumed to be susceptible.
- 9.) The shrub, vines and groundcover lists are limited. The rationale for selecting these species is not evident. A statement should be added which says that this list is not all-inclusive but represents reliable and available plant selections.

I would be happy to assist in any revisions of the plant lists. Please contact me if I can be of further assistance.

Respectively submitted,

  
James MacNair

cc: Hal Beck, Sonoma Chamber of Commerce  
Linda Gates, Gates and Associates

September 19, 1994

Mr. Tom Bane  
Redevelopment Manager  
Sonoma County Community Development Commission  
1440 Guerneville Road  
Santa Rosa 95403

Dear Mr. Bane,

I want to thank you for having the opportunity to lend my support to the "Highway 12 Design Guidelines" presented in August at the Sonoma Valley Community Advisory Commission meeting.

As a relatively new owner -- one and a half years -- of a beautiful 70-year-old wood frame "resort" style home in Boyes Hot Springs, I have awaited the presentation by the designers who were charged with devising the guidelines to improve the Springs corridor along Highway 12. I bought my house, which I have continued to upgrade, with the expectation that this plan would aid the Springs in flowering with a bit of common sense by Sonoma County and its residents.

Now I applaud the vision of David and Linda Gates that is reflected in the guidelines. They have shown that they certainly deserved to win the contract among the 18 bidders. It's an excellent roadmap to use, right on target. Creating an identity for the corridor, taking advantage of the Mediterranean flavor of the wine country and the Inn as well as taking into account the historic Latino traditions and aesthetic is just what we all want. There was enthusiastic support at the meeting for the guidelines as well as a grave impatience with the continuation of the blight.

The designers were quite correct in recognizing that the route is well used by many on-foot residents who of course see it as part of their neighborhood.

Particularly interesting -- and clever -- is the creation of the "main street," "country commercial" and "corridor residential" concepts.

In the Boyes Boulevard Study Area, which has the greatest potential for beauty yet is a tragic picture of neglect by the county, I support the creation of a Town Center Plaza.

Residents -- and visitors -- naturally gravitate to the U.S. Post Office and the services especially in the blocks of Vallejo, Central and Monterey avenues on the east side of the highway and Boyes Boulevard on the westside near the Sonoma Mission Inn.

Thus, the plan for a design on a pedestrian scale with upgraded sidewalks, shallow setbacks, bike paths and walk ways fits the bill.

Needless to say, the highway must be restricted to three lanes.

We are all in agreement that during the uncertainty of whether Cal-Trans was going to widen Highway 12, the corridor was allowed to degenerate. Before I bought my house, I was assured by Cal-Trans that it had no plans to widen 12 (or I wouldn't have purchased my house). Now the county has an opportunity to turn the road into the scenic highway that it has designated.

I support the most speedy adoption of the Highway 12 design guidelines by the Sonoma County Board of Supervisors.

I hope that we can take advantage of the redevelopment and other budget monies to get this project in motion before the end of 1994.

Sincerely,

A large black rectangular redaction box covering the signature and name of the sender.





A Community Project of the  
Sonoma Valley Chamber of Commerce

645 Broadway  
Sonoma, CA 95476  
Tel. 707-996-1033  
Fax 707-996-9402

8/25/94

To: Tom Bane, SC Redevelopment  
From: Hal Beck, SVCofC  
Re: Draft Report - final comments

FAX 524-7557

1. Yes you know Micheal Cale's name is wrong as is Fred Gerdes.
2. I believe the top photo on page 10 is reversed.
3. The benches shown on page 63 need to have an arm in the middle, to preclude non-appropriate usage.
4. The palms listed on page 67 are also suitable for "Country Commercial" per James MacNair, who helped put the City's tree list together.
5. Page 54, section E.1 "Avoid wood fences along Hwy.12 except for low ornamental fencing such as picket fences." What was used in the new parking lot???

Thank you.

[Redacted signature]

cc. David & Linda Gates

*Our Mission:*  
*Improving the appearance of the unincorporated area of*  
*Sonoma Valley by promoting a more positive business environment*  
*and enhancing the overall quality of life.*

Tom Bane  
Community Development  
May 11, 1994

MARLAINA PIPAL  
848 1st St. West  
SONOMA, CA 95476  
Ph. 938-9359

Dear Tom,

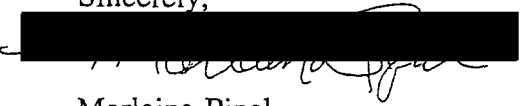
I called to inquire about the planning of the Boyes Hot Springs-Highway 12 community. As a resident of El Verano, a commuter, a parent, a bicyclist and a landscape architect, I would like to participate in the design process to improve this neighborhood.

First of all, please observe that the existing trees which shape and shade the hot pavement and structures here are Valley Oak and Coast Live Oak trees. I contend that lining Hwy. 12 with more of them would maintain the California character of Boyes. I could suggest other species of trees as specimens for shade, fall color (pistacia chinensis) or blossoms, but oaks will grow well here. Crepe Myrtles provide a burst of color in July and August, but create minimal shade. Another variety of Mediterranean oak to consider is the cork oak, 60' tall as seen in front of 4 schools in the Sonoma Valley. I have specified planting plans for several commercial buildings in Sonoma and am familiar with what looks good and thrives in this area.

I hope that the future improvements include safe and comfortable places for children who walk and bike to school at Flowery, Altamira or to Larsen Park, benches and shade at the bus stops, public drinking fountains, drive up postal mailboxes, bilingual signs, pedestrian pockets, incentives to reduce single passenger commutes and better landscaped street fronts of the businesses that line this avenue into the Sonoma Valley.

Anyway, as you can see, I care about improvements for the residents, the businesses and the visitors in Boyes Hot Springs, so please keep me posted about the next meeting.

Sincerely,

  
Marlaina Pipal

Aug 31, 1994

Dear Tom Bane,

At the Sonoma Valley Citizen's Advisory Commission meeting in August, Gates & Assoc. presented the draft of the Highway 12 Design Guidelines.

I noticed that the two creeks of that segment of Hwy 12 were not mentioned as a design component.

You suggested I write you with more info. Enclosed please find photos of the creeks and surrounding corridor with notations.

It is my hope that both of these creeks are treated with respect when Hwy 12 is developed and the utilities are undergrounded. It is difficult to restore watersheds, it is much easier to act with the foresight of leaving a buffer of vegetation around these creeks.

Agua Caliente Creek, a major tributary of the Sonoma Creek, doesn't look like much more than a dry wash in the summer of this 8th year of drought. However, in the past it has held deep pools through the summer. It deserves a sensitive treatment. It is also a riparian

corridor - specifically an animal corridor - from the hills of the east to Sonoma Creek. It is one of the few ways animals can get to the creek without going across Highway 12.

The smaller creek - though not as important a corridor of course - has a special place in my heart because I have lived by it for twenty years. I hope implementation of design guidelines with an added creek treatment component will bring out the charm of Pequeno Creek and provide enjoyment for pedestrians, both local and visitors.

I hope this report is helpful. I wish I could have had the time to print it out on a Mac, but so it goes.

Sincerely,  
Christy Vreeland  
17102 Park Ave  
Sonoma, Ca 95476

member - Verano/Springs Association (Secretary)  
member - Sonoma Valley Watershed Council (Project Chair)

Aug 31, 1994

## CREEKS/HWY 12 Redesign Report

There are two creeks within the Highway 12 Design Guidelines area.

The larger creek, Agua Caliente Creek, is a major tributary of Sonoma Creek. Starting high in the eastern hills, Agua Caliente Creek is several miles in length; development along the creek starts only a  $\frac{1}{4}$  mile to the east of Hwy 12. When not full from winter storms it serves as a pathway for animals from Sonoma Creek to the eastern wilderness areas.

The smaller creek originates in the eastern hills above the Pequeno Valle Ranch at the top of Park Avenue. It crosses Hwy 12 south of the "Mountain Ave Study Node" near Fellers Avenue and flows into Sonoma Creek through Larsen Park. It has no formal name, for purposes of this report it will be called Pequeno Creek.

C. Vreeland.

## Specific recommendations

- > Signage of the creeks
- > Landscaping choices - use native trees and shrubs to retain riparian integrity
- > Public viewing area such as bench under trees.
- > When bridges are rebuilt leave adequate space for creek to find a natural channel as much as feasible
- > When bridges are rebuilt factor in heavy pedestrian traffic & provide safe walkways.
- > creek & near creek areas are especially suitable for sidewalk meander design



Pequeno Creek at Hwy 12  
Eastside, facing south



right over grade  
Southern GATEWAY  
STUDY AREA



Current pedestrian/bikeway at Agua Caliente Creek  
westside

## AGUA CALIENTE CREEK

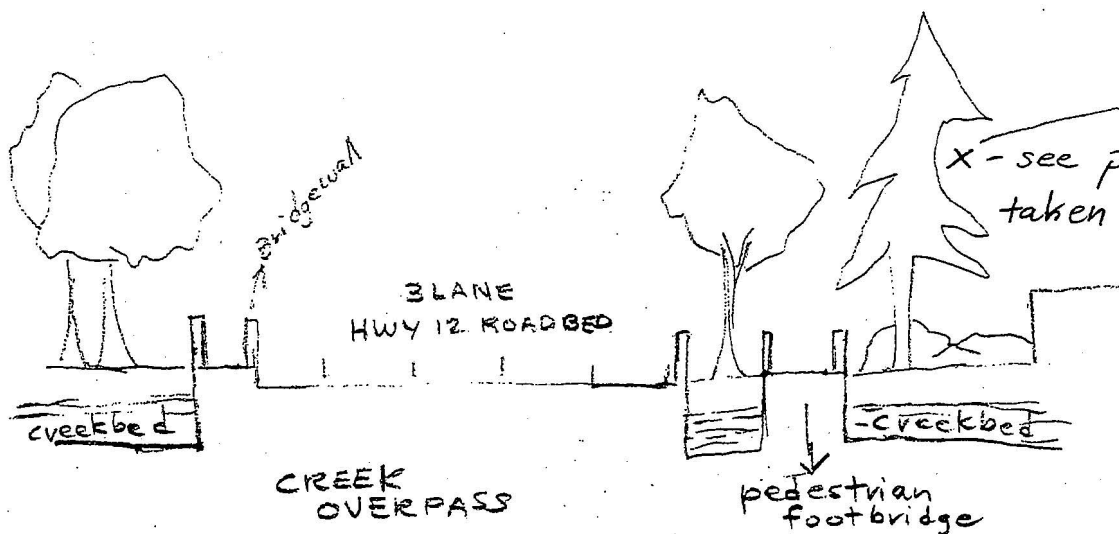
On eastside, creek turns and follows 12 to  
Brookside Mobile Home Driveway.  
Note the pedestrian walkway is even more constricted





Pequeno Creek at Highway 12, Westside, looking south.

- > Right now folks encounter Pequeno Creek mainly as a constriction of the roadway, forcing pedestrians and bicyclists closer to auto traffic. When the street is widened to 3 lanes, the bridge will be rebuilt. (The current bridge dates from 1924 & was built with an opening about half the winter flow of the creek)
- > A pedestrian bridge pathway separate from the road way would be compatible with the "meander" concept for sidewalks in County Commercial and Residential Corridor guidelines.





AGUA CALIENTE CREEK  
SUMMER 1994



Highway 12 facing north

Modern Plumbing

"Mountain Ave Node  
Flowery School crossing



Creek crossing Hwy 12



Pequeno Creek meanders through this vacant lot which fronts on Hwy 12. Retail businesses are to the north and south of the lot. The southwest corner of the lot is being used as a children's play area with a treeswing.

See previous photo, x marks spot this photo was taken.